

BZA Application #19169C

Request for Modification of Significance under ZR-16

303 – 317 K Street NW

Birchington LLC

Presented by:

Meridith H. Moldenhauer

Cozen O'Connor



Board of Zoning Adjustment

District of Columbia

CASE NO: 19169C

Exhibit No : 41

Board of Zoning Adjustment

District of Columbia

CASE NO: 19169C

EXHIBIT NO. 41



Summary of Modification of Significance Application

- To amend March 2016 approval of BZA Case No. 19169 (the “2016 Approval”) of 130’-tall hotel/residential project with 200 hotel rooms and 30 residential units in the (then) DD/DD-HPA/C-2-C Zone District.
 - 2016 Approval issued under ZR-58 and granted variance relief necessary for that development under ZR-58: one 30’-loading berth, rear yard & parking
- Current Modification Application for all-hotel project (247 units) with similar height, massing, 42 parking spaces and two service spaces in a below-grade garage accessed from new 4th Street curb cut that requires DDOT approval through the Public Space Committee.
- Current Modification Application is processed under ZR-16 and requires special exception relief for:
 - Two, 30’-loading spaces (Subtitle C § 909.2); and
 - 14% driveway grade (Subtitle C § 909.3)

Charts Identifying Differences Between Relief in 2016 Approval and Current Modification Application

2016 Approval Relief Granted: BZA Case No. 19169

Type of Relief	Required (under ZR-58)	Provided	Relief Granted
Rear Yard § 774.1	15'	0	100% rear yard relief
Parking § 2101.1	114 spaces	46 spaces	From 76 spaces
Loading § 2201.1	One 30'- loading berth One 100'-loading platform One 20'- service space	One 20'-service space	One 30' loading berth One 100' loading platform

Modification of Consequence Relief Requested: BZA Case No. 19169C

	Required (under ZR-16)	Provided	Relief Requested
<u>Loading</u> Subtitle C §§ 901.1; 904.1	Three 30'- loading berths + platforms Driveway slope of 12%	Two 20'-service spaces Driveway slope of 14% (Approval for one 30'- berth granted in 2016 Approval)	Two 30'-loading berths + platforms Driveway slope of 14%

List of Important Exhibits in the BZA Record

- BZA Exhibit No. 8: Self-Certification/Form 135 (Revised 5/29 to reflect necessary ZR-16 relief)
- BZA Exhibit No. 9: Applicant Statement (Revised 5/29 to reflect necessary ZR-16 relief)
- BZA Exhibit No. 10: Written Order of 2016 Approval
- BZA Exhibit Nos. 11-12: Plans approved in the 2016 Approval
- BZA Exhibit No. 14: 2018 Written Order Granting a Two-Year Time Extension for the 2016 Approval
- BZA Exhibit No. 17: Includes 2016 Approval DDOT Report (Tab J) and 2016 Approval Transportation Study (Tab M)
- BZA Exhibit No. 31: ANC 6E Report in support of Modification Request
- BZA Exhibit No. 35: DDOT Report in support of Modification Request
- BZA Exhibit No. 37: ZA confirmation of Special Exception relief for loading (Subtitle C § 909.2) & Applicant's explanation of why Special Exception relief is appropriate
- BZA Exhibit No. 38: OP Report in support of Modification Request
- BZA Exhibit No. 39: Project Plans for Modification Request

Zoning Map

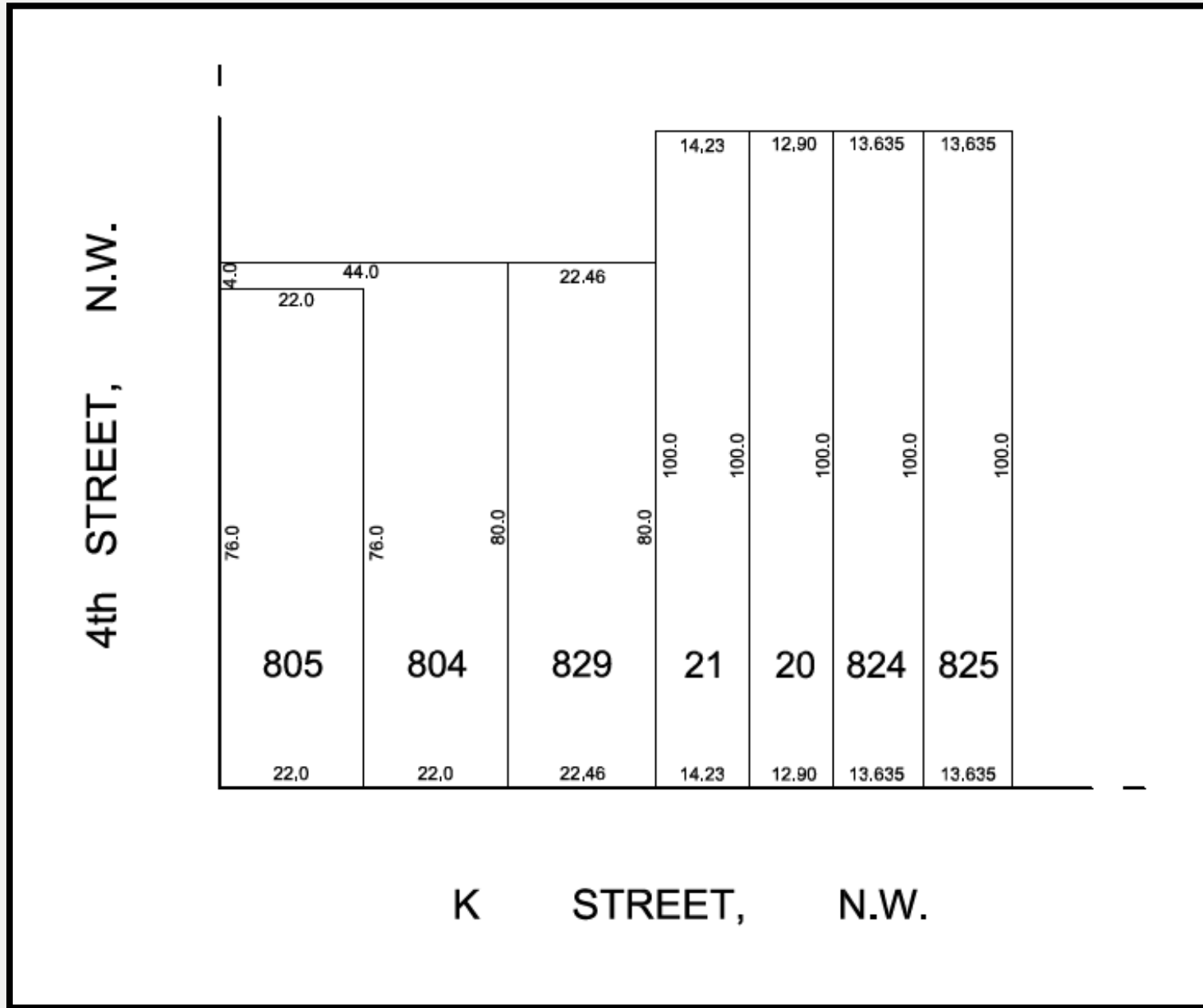


Square 526

**Lots 20 – 21,
804 – 805,
824 – 825, 829**

D-4-R Zone

Plat



Current Images of the Property



2016 Approval (BZA 19169)

- **February 23, 2016**: BZA grants the BZA Application in Case No. 19169 (“2016 Approval”)
 - ANC, OP, DDOT all support the Original Approval

- Written Order in the Record at **BZA Exhibit No. 10**

2016 Approval (BZA 19169)

2016 Project:

- 130'-tall, 11 FAR; hotel/residential building with 200 hotel units and 30 residential units and rear closed court
- 46 parking spaces in below grade parking garage from 4th Street curb cut
- One 20'-service space, but no 30'-loading berths possible; DDOT and OP support a 50'-on-street loading/valet space on 4th Street
- Transportation Demand Mitigation and Loading Management Plans Approved

2016 Approval (BZA 19169)

- Relief Granted in 2016 Approval:
 - Variance for 100% rear yard relief
 - Variance for parking relief for 76 spaces
 - Variance for loading relief for one 30'-loading berth and platform
- March 29, 2018: BZA grants a two-year extension to the 2016 Approval (BZA Case No. 19169A).
 - Extension approval supported by the ANC and OP.
 - Written Order approving the two-year extension in record at BZA Exhibit No. 14

Modification Request (BZA 19169C)

Modified Project:

- Essentially same massing as the Original Project, but all-hotel uses and more efficiently designed
- 130'-tall, 10.4 FAR
- 247 hotel rooms
- 42 parking spaces in below-grade parking garage from 4th Street curb cut
- Two 20'-service spaces below grade, but no 30'-loading berths possible; Applicant to separately apply for a 50'-on-street loading/valet space on 4th Street
- Applicant to implement the Loading Management Plan in the Record at BZA Exhibit No. 34.

Comparison of 2016 Approval and Modification

	2016 Approval	Modified Project	Change/ Additional Relief in Modification Request
Uses	200 hotel rooms/ 30 residential units	247 hotel rooms	All-hotel project
Proposed Building GFA	117, 846 s.f. of gross floor area	111,604 s.f. of gross floor area	- 6,243 s.f.
Proposed FAR	11.0 FAR	10.4 FAR	- 0.6 FAR
Height	130'	130'	No change
Closed Court	2,054 s.f.	2,166 s.f.	+ 112 s.f. in court size
Penthouse Height	18'-6"	18'-6"	Modified Project penthouse will include indoor meeting/function space (no bar/restaurant)
Rear Yard Setback	None	None	Rear Yard relief granted in 2016 Approval (Not required in ZR-16)
Side Yard Setback	None	None	No change
Parking spaces provided	46	42	Parking relief granted in 2016 Approval (None required in ZR-16)
30'-loading berths	None	None	Relief granted for one 30'-loading berth in 2016 Approval. Relief required for two 30'-loading berths for Modified Project (total of three loading berths required under ZR-16)
20'-Service Spaces	One	Two	Additional 20'-service space included in the Modified Project
Bike Spaces	None	14	14 bike spaces in the Modified Project
GAR	30% of the site	30% of the site	No change
Driveway Grade	12%	14%	Relief required for 14% driveway grade; for Modified Project (12% grade required)

Side-by-Side Comparison: Proposed Renderings

APPROVED DESIGN



PROPOSED DESIGN



Side-by-Side Comparison: Building Information

APPROVED ZONING & FAR

Floor and GSF	HOTEL		RESIDENTIAL		FAR	CONSTRUCTION
	Parking Adm. PAR.	Total Sleeping Rooms	Total Residential Units	Total Area Residential G.A.F.		
First Floor			10	8,238	8,248	8,238
Second			10	8,129	8,139	8,129
Third			10	8,129	8,139	8,129
Fourth			10	8,129	8,139	8,129
Fifth	38	8,321	33	8,447	8,481	8,481
Sixth	38	8,321	33	8,447	8,481	8,481
Seventh	38	8,321	33	8,447	8,481	8,481
Eighth	38	8,321	33	8,447	8,481	8,481
Ninth	38	8,321	33	8,447	8,481	8,481
Tenth	38	8,321	33	8,447	8,481	8,481
Eleventh	38	8,321	33	8,447	8,481	8,481
Twelfth	38	8,321	33	8,447	8,481	8,481
Thirteenth	38	8,321	33	8,447	8,481	8,481
Fourteenth	38	8,321	33	8,447	8,481	8,481
Fifth Floor		8,311		8,447	8,481	8,481
Garage 1	18,737					18,737
Garage 2	18,737					18,737
Total	25,852	280	107,398	907,480	112,448	926,278

PROPOSED ZONING & FAR

FLOOR	FAR and GSF		FAR	GSF
	KEYS	NON FAR		
Garage 2		10,737 SF		10,737 SF
Garage 1		10,737 SF		10,737 SF
Ground			8,450 SF	8,450 SF
Second	18 KEYS	218 SF	8,262 SF	8,600 SF
Third	21 KEYS	218 SF	8,692 SF	8,730 SF
Fourth	21 KEYS	218 SF	8,692 SF	8,730 SF
Fifth	21 KEYS	218 SF	8,692 SF	8,730 SF
Sixth	21 KEYS	218 SF	8,692 SF	8,730 SF
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Eleventh	21 KEYS	218 SF	8,692 SF	8,730 SF
Twelfth	21 KEYS	218 SF	8,692 SF	8,730 SF
Thirteenth	21 KEYS	218 SF	8,692 SF	8,730 SF
14th Floor		11,7 SF	8,472 SF	8,589 SF
15th Floor		2,862 SF		2,862 SF
16th Floor		273 SF		273 SF
TOTAL	247 KEYS	2,714 SF	112,430 SF	118,544 SF

- RESIDENTIAL USE REMOVED
- HOTEL USE INCREASED
- FAR REDUCED
- 1 FLOOR REMOVED

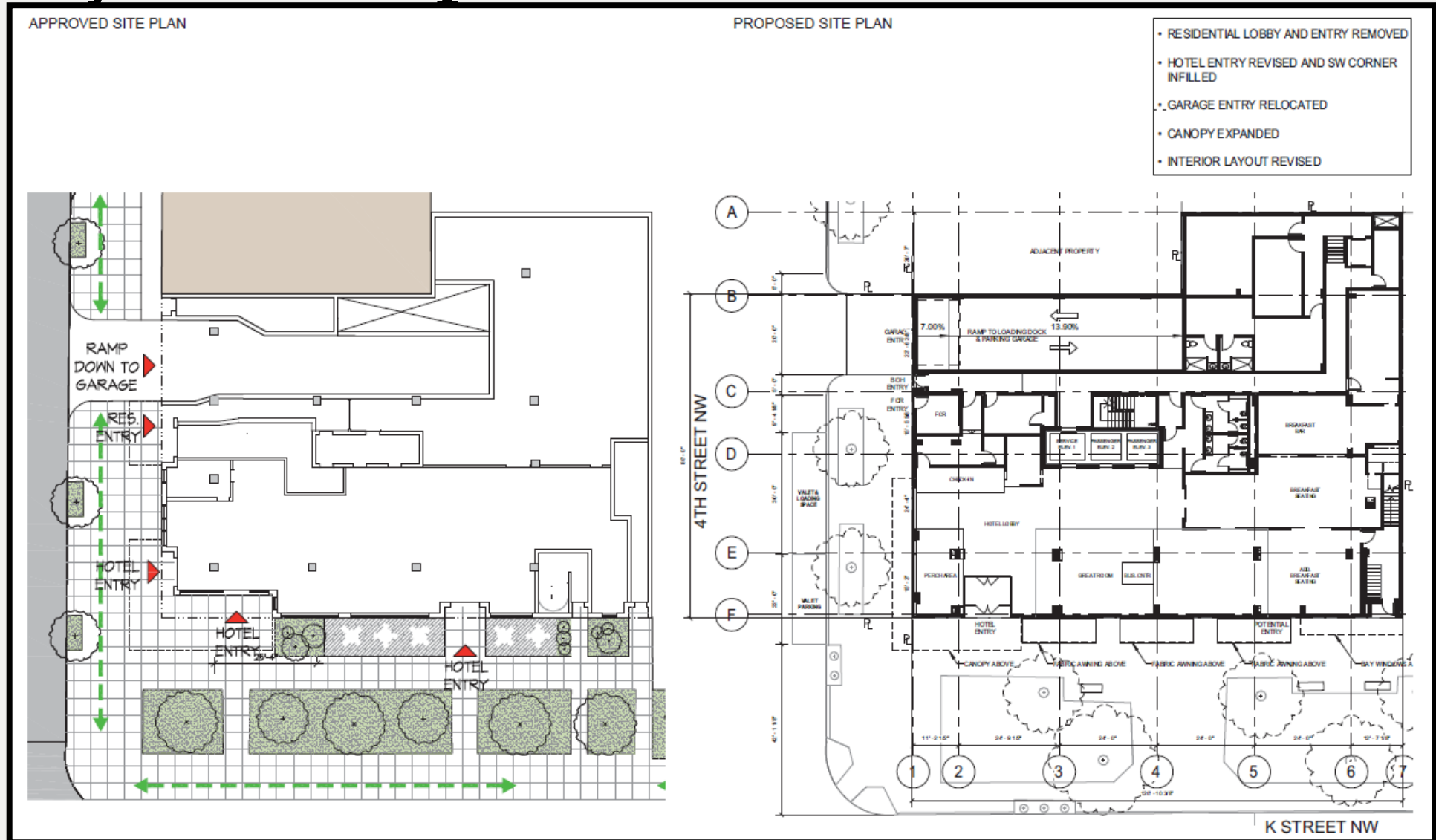
JOBING REQUIREMENTS	REQUIRED / ALLOWED	PROVIDED
Strong District	C-24 / (D) (Development Density) / CC-FPA (Planning Priority Area A)	CC-2 / (D) (Development Density) / DD-FPA (Planning Priority Area A)
Lot Area	10,707	10,707
FAR Total Hotel/Residential (Max. or Precedent) (1)	11.0	117,927
Indicate FAR Required		
Max Residential FAR (Max)	3.5	37,480
Max Hotel FAR (Max)	3.5	37,480
Transfer of Contribution to Affordable Housing (Hotel)	0.0	10,307
Subtotal (Hotel)	0.0	88,156
Some Financial Contribution to Affordable Housing or TRS (Hotel)	0.5	5,244
Residential FAR (Hotel)	0.5	51,527
Residential FAR (Hotel)	0.5	26,477
TOTAL FAR	11.0	117,927
Lot Coverage		
Ground Floor	100%	10,707
Upper Hotel Floor (2-11)	100%	10,707
Typical Residential Floor (2-14)	96%	8,800
Height	132'-0"	130'-7"
Ground Court at Hotel Floor 1-11 (Height of Court - 10'-0")	Minimum 20' x 20' (Height of Court - 10'-0")	35'-0" x 4'-0" (Height of Court - 10'-0")
Ground Court at Hotel Floor 1-11 (Height of Court - 10'-0")	Minimum 20' x 20' (Height of Court - 10'-0")	35'-0" x 4'-0" (Height of Court - 10'-0")
Front Yard Setback	15'-0"	None Provided (Ratio Provided)
Side Yard Setback	None required	None
Rear Yard Setback	10'-0"	None Provided (Ratio Provided)
Side Hard Setback	None required	None
Hardwood Height	10'-0"	10'-0"
Hotel Parking (Based on Hotel 2-11) (100,000 sq ft) (100,000 sq ft) (100,000 sq ft)	1 space per 2 rooms plus 1 for each 100,000 sq ft of floor area for larger hotel (100,000 sq ft) (100,000 sq ft) (100,000 sq ft)	35 Provided (Ratio Provided for TR)
Residential Parking (Based on Hotel 2-11) (100,000 sq ft) (100,000 sq ft) (100,000 sq ft)	1 to each 1,000 sq ft of space (100,000 sq ft) (100,000 sq ft) (100,000 sq ft)	8 spaces
Total Parking	Residential + Hotel	43 Spaces
Hotel Loading Docks	1 @ 30'-0" Deep	None Provided (Ratio Provided)
Hotel Loading Platform	1 @ 100 SF	None Provided (Ratio Provided)
Hotel Loading Platform	1 @ 100 SF	None Provided (Ratio Provided)
Hotel Service/Delivery Loading Space	1 @ 20'-0" Deep	None Provided (Ratio Provided)
Residential Loading, Bikes, Platform Space	None required	None
Hotel Bikes	None required	None
Residential Bikes	1 space per 2 units. Required 10	34
VEP	Apply 10% area to be vegetated	Apply 10% area to be vegetated

¹ Applicant requests 11.0 FAR. 26.000 is a preliminary opinion. 26.000 is a preliminary opinion. 26.000 is a preliminary opinion. 26.000 is a preliminary opinion.

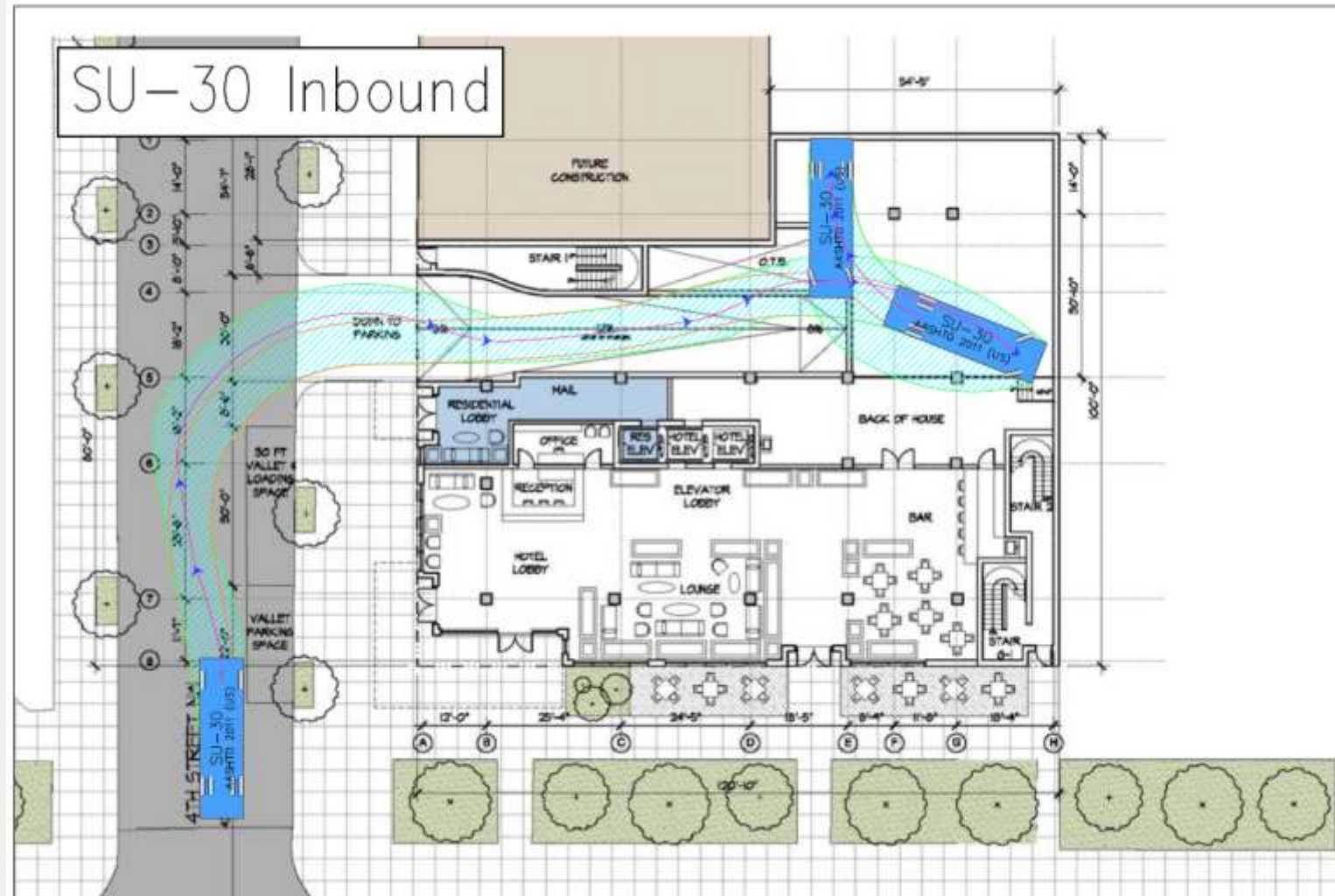
JOBING REQUIREMENTS	28-16 REGULATIONS	
	REQUIRED/ALLOWED	PROVIDED
zoning District	D-4-D	D-4-D
Lot Area	10,707	10,707
FAR Total Hotel (Max. or Precedent) (1)	11.0	117,927
Indicate FAR Required		
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Hotel Service/Delivery Loading Space	1 @ 20'-0" Deep	None Provided (Ratio Provided)
Residential Loading, Bikes, Platform Space	None required	None
Hotel Bikes	None required	None
Residential Bikes	1 space per 2 units. Required 10	34
VEP	Apply 10% area to be vegetated	Apply 10% area to be vegetated



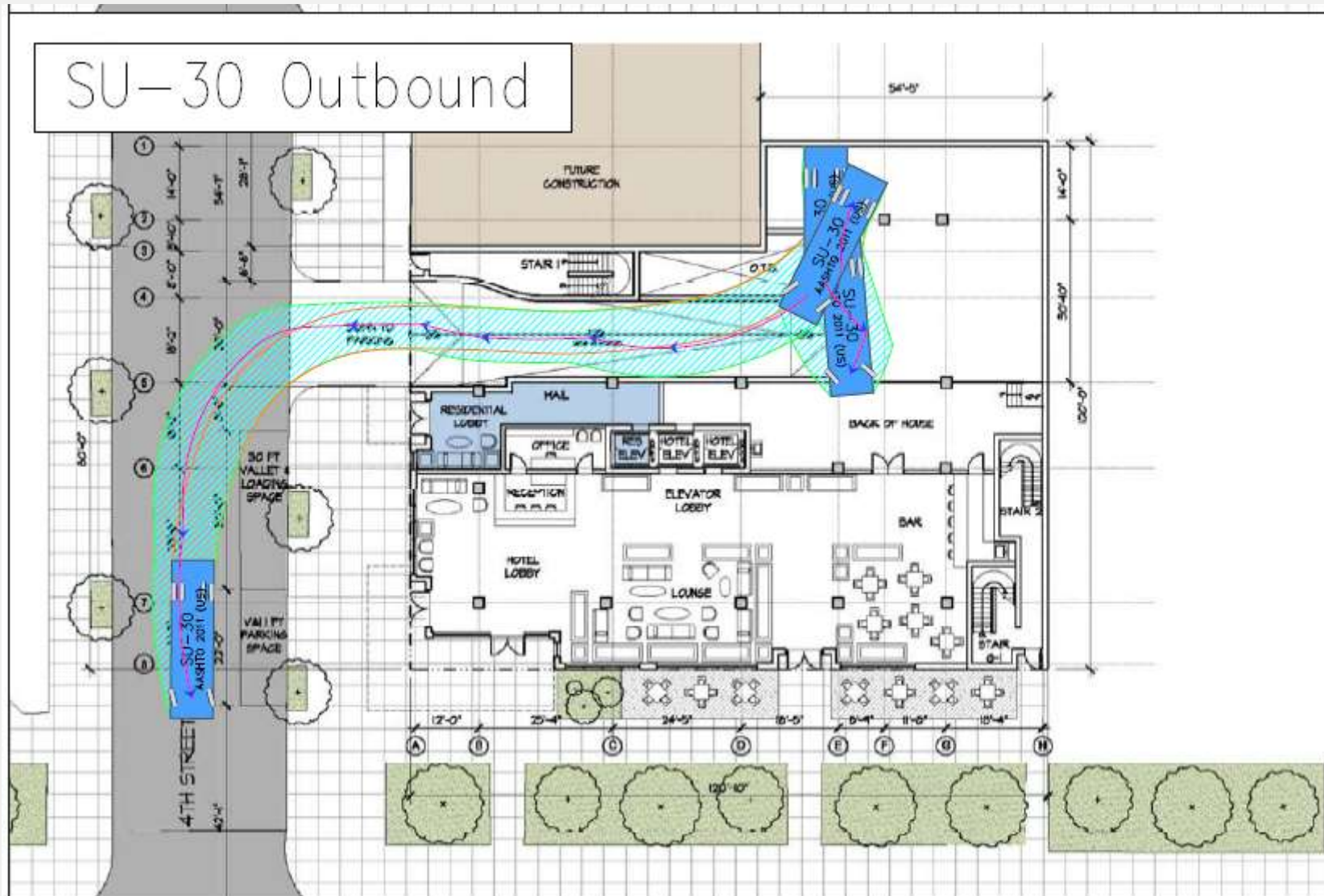
Side-by-Side Comparison: Site Plan



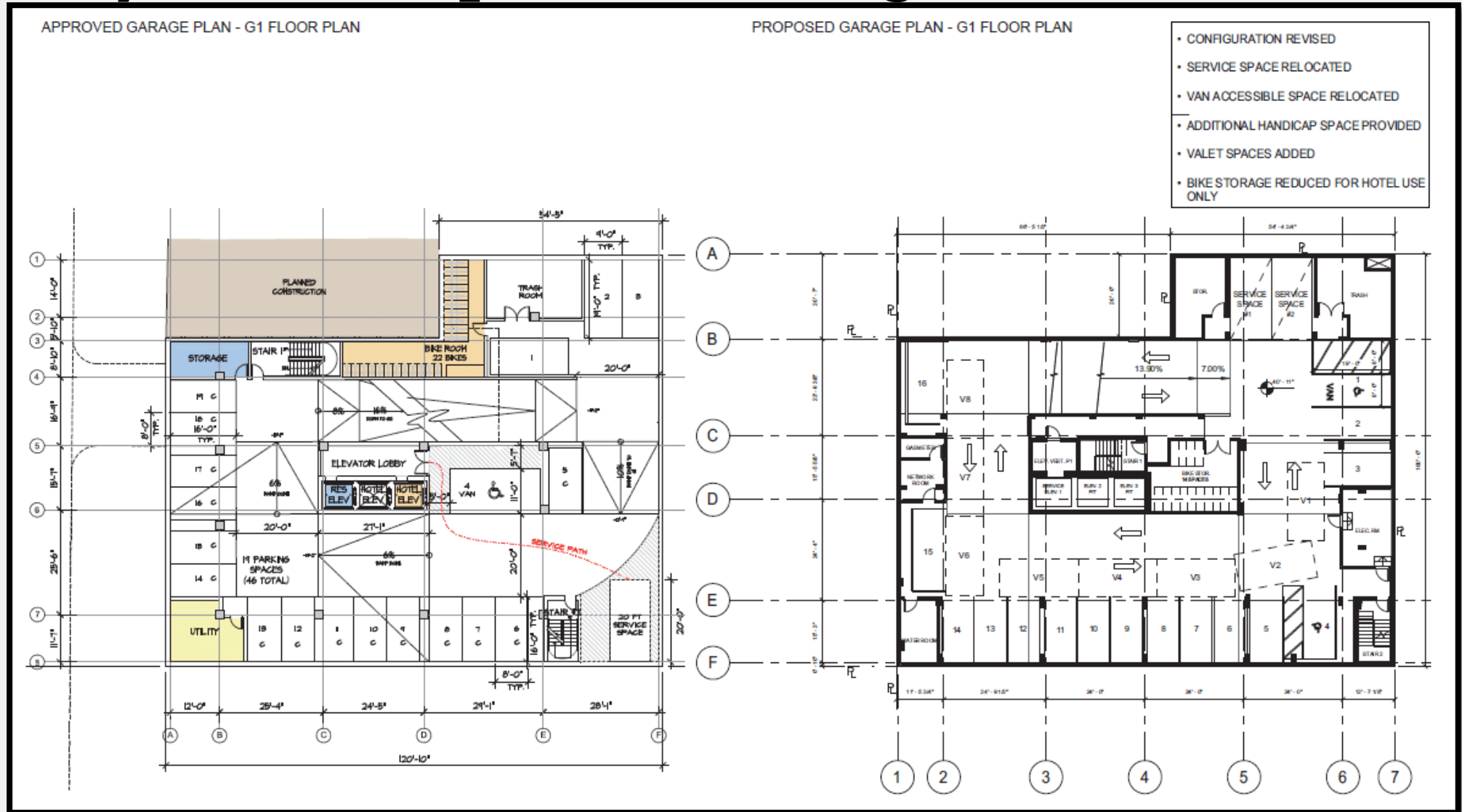
No 30'-Loading Berth Possible On the Site with Front-in Loading



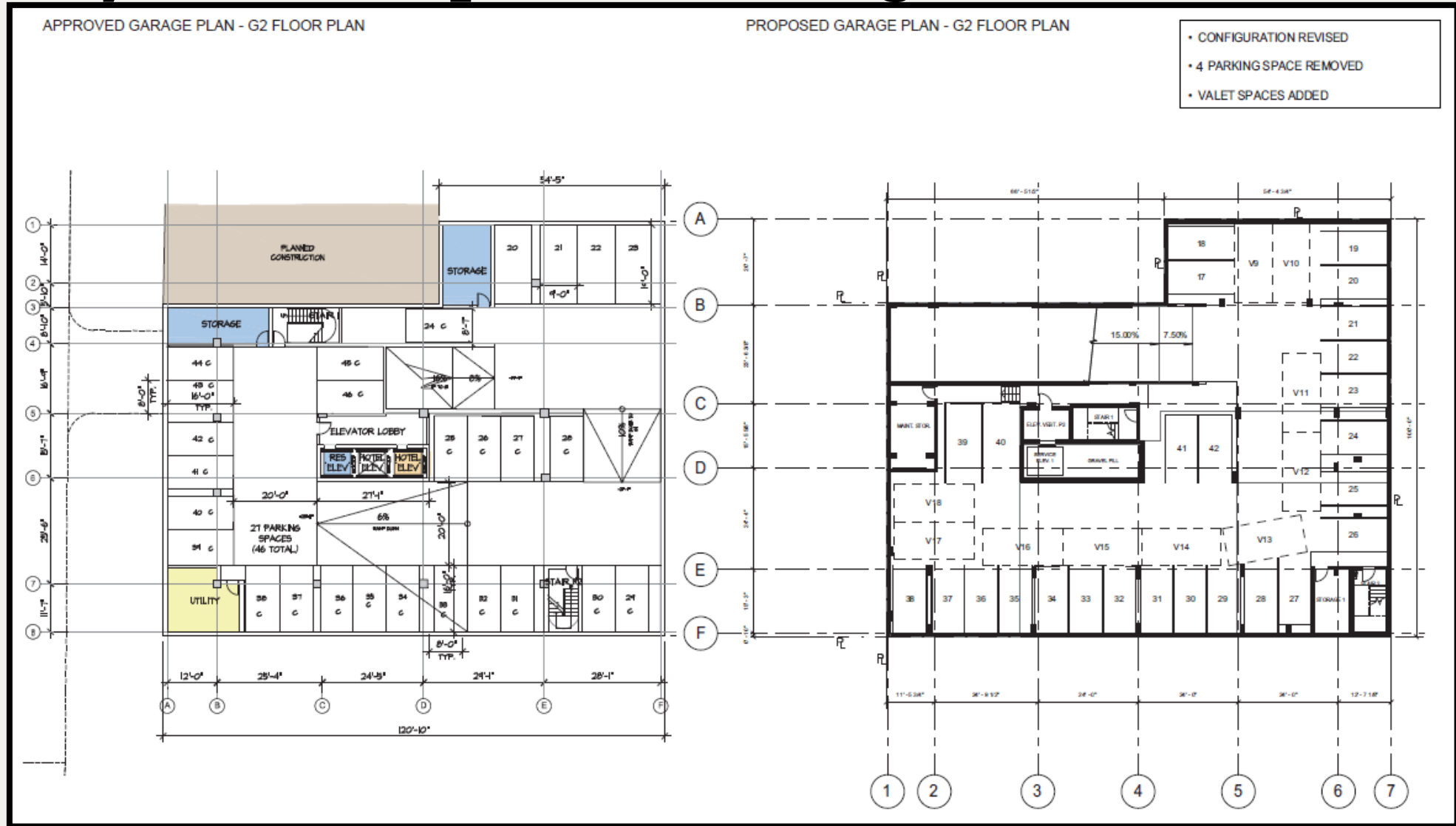
Similarly, front-out loading is not possible



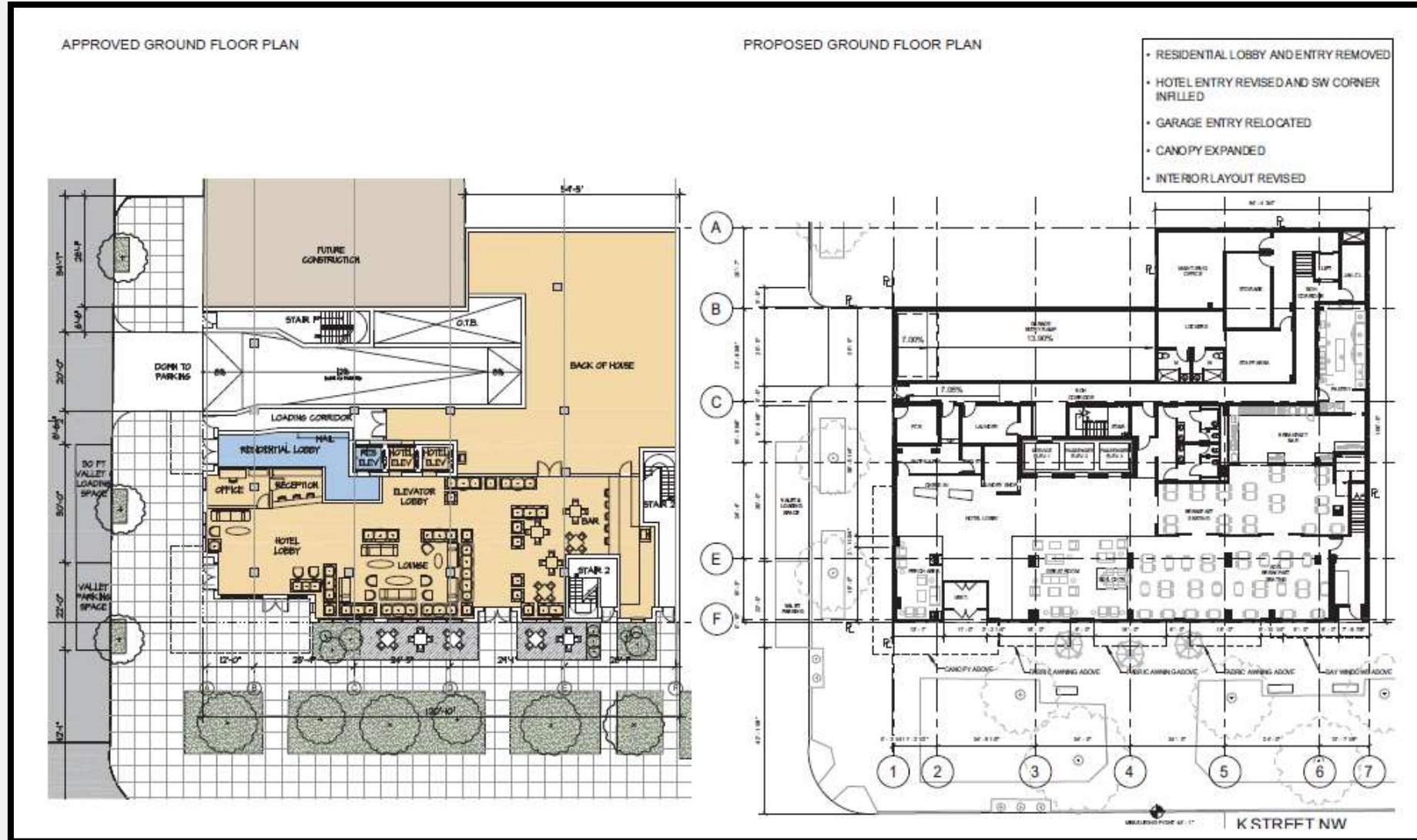
Side-by-Side Comparison: Garage Floor Plan, G1



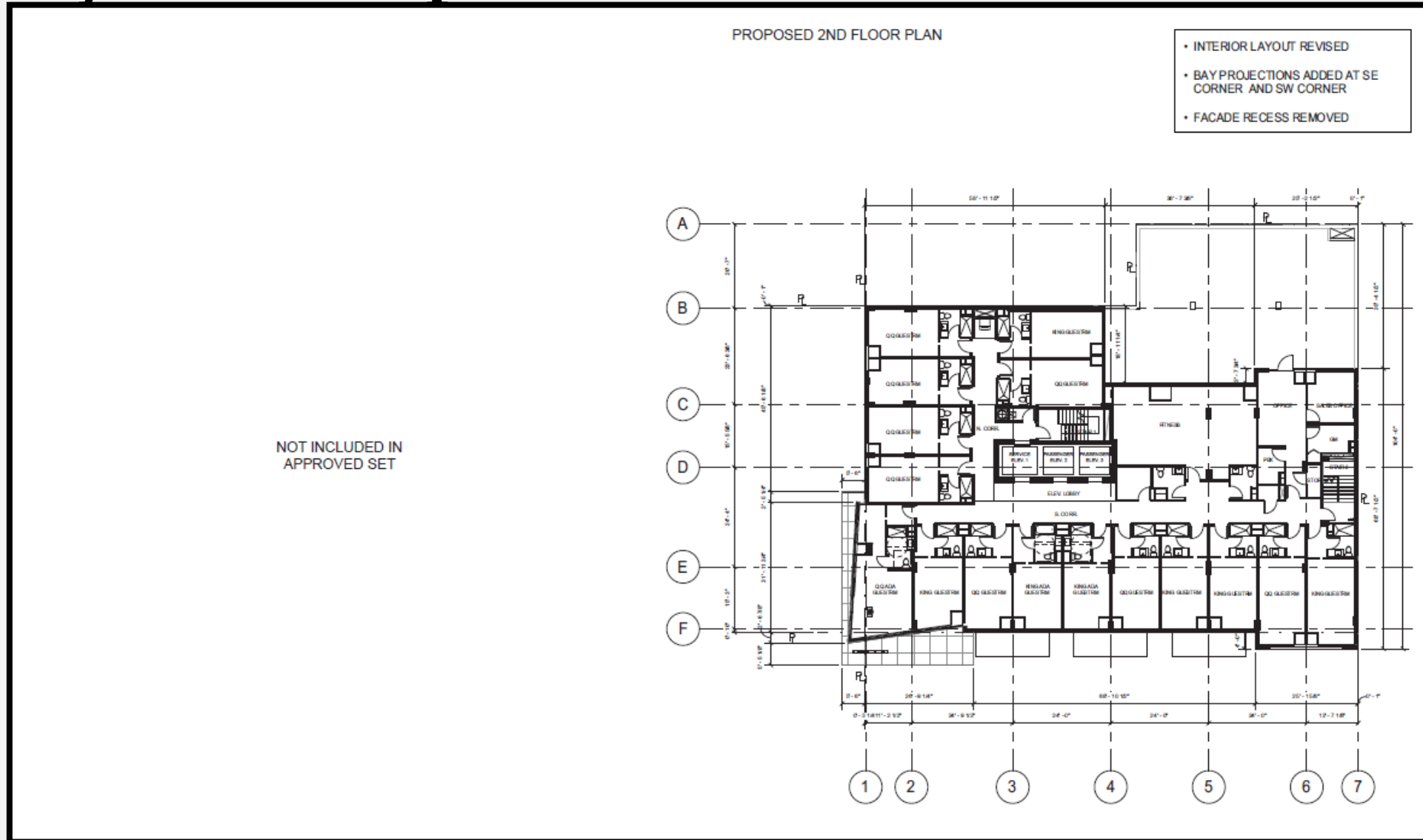
Side-by-Side Comparison: Garage Floor Plan, G2



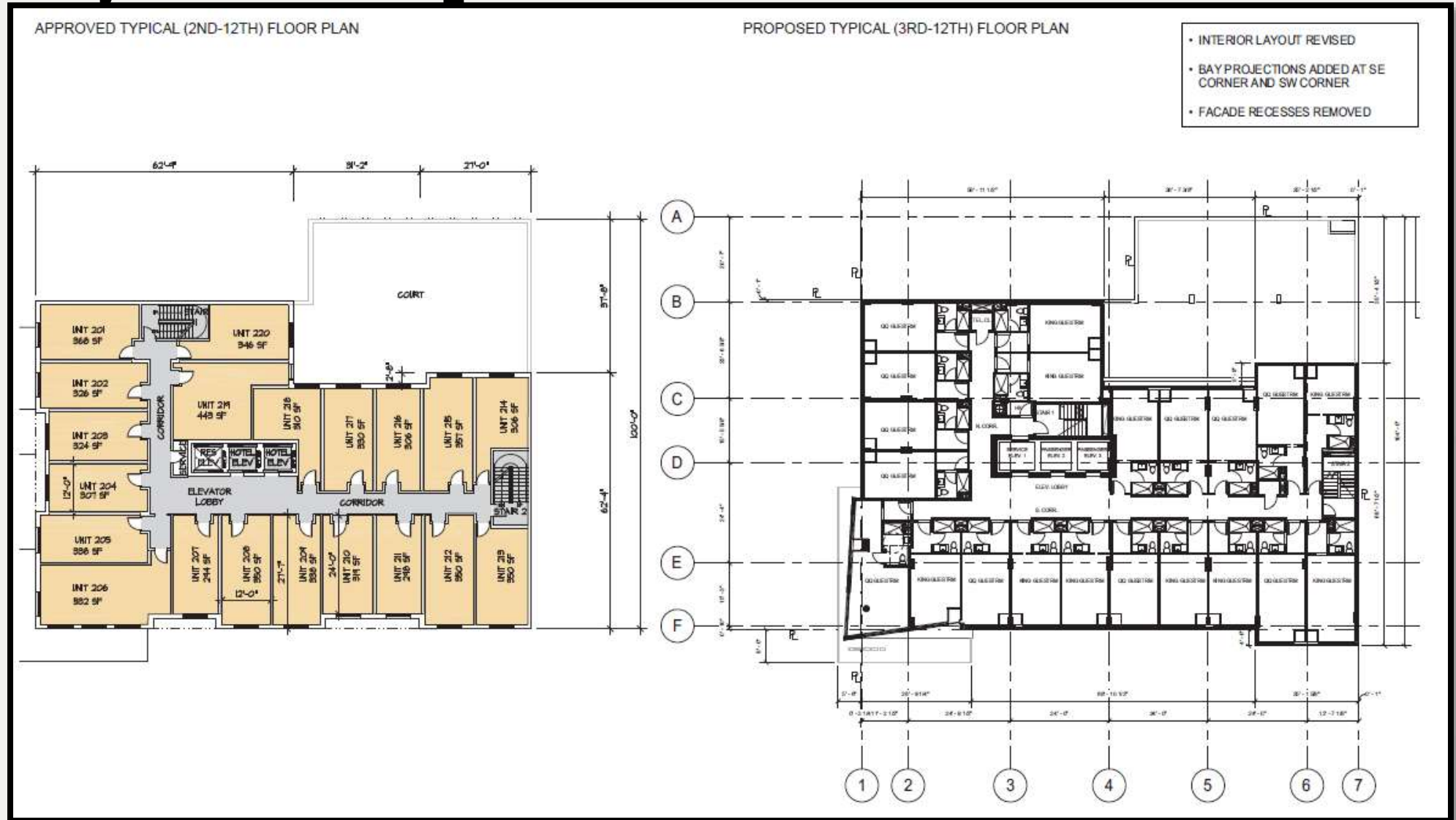
Side-by-Side Comparison: Ground Floor



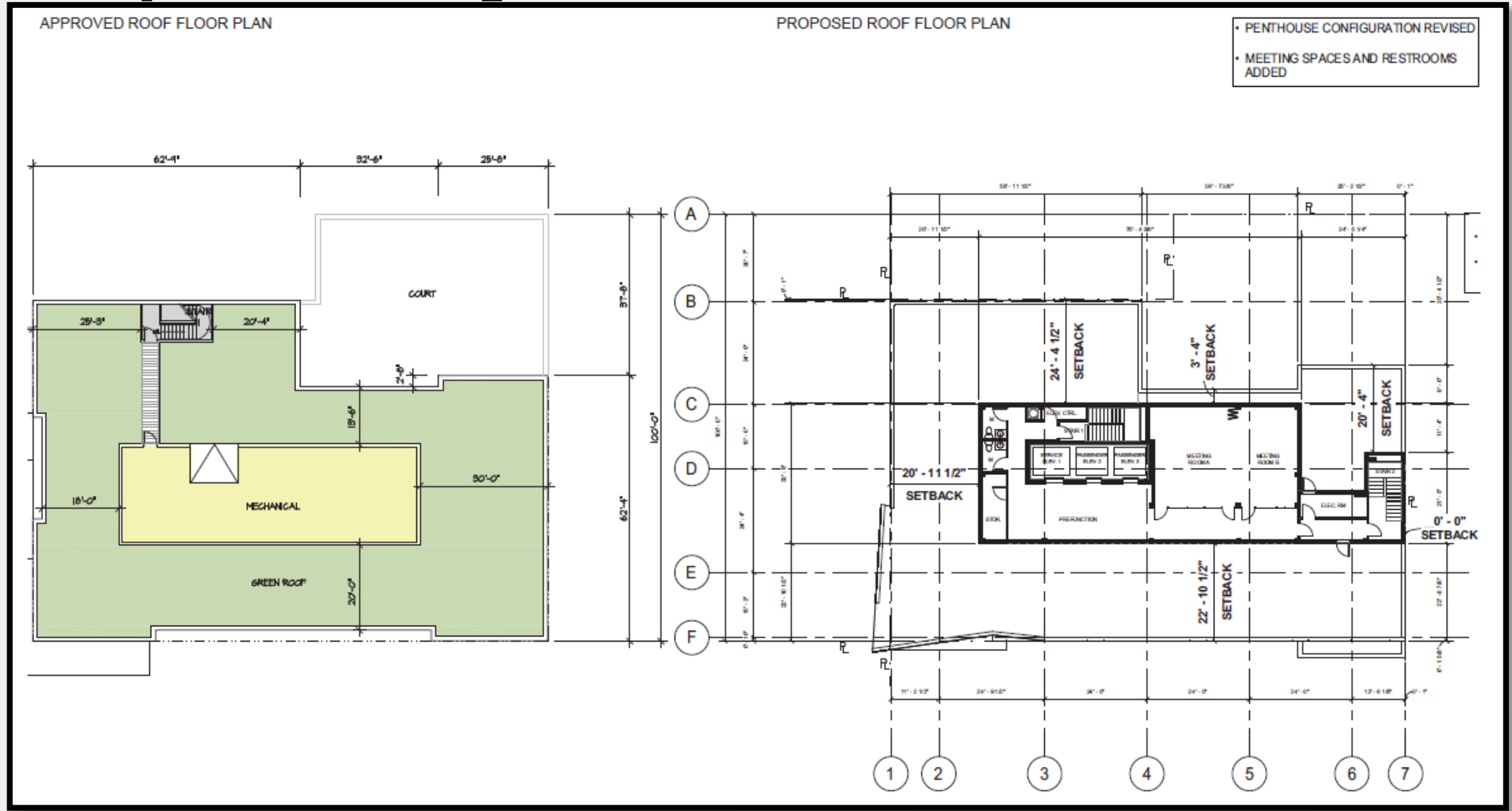
Side-by-Side Comparison: Second Floor



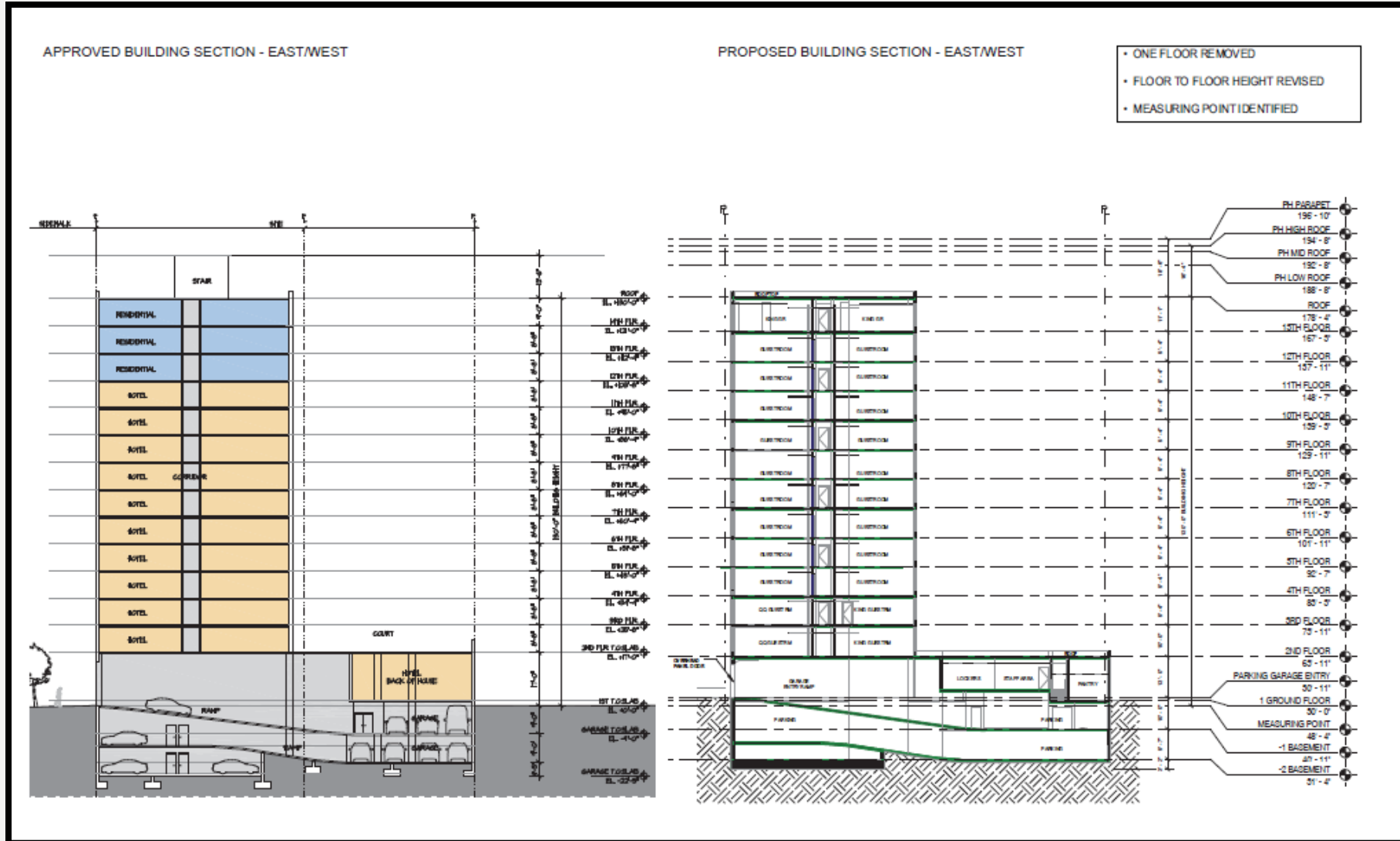
Side-by-Side Comparison: Floors 2/3 – 12



Side-by-Side Comparison: Roof



Side-by-Side Comparison: East/West Building Section



Special Exception Relief Requested

Subtitle C § 901.1

Loading Requirements for two, 30'-loading berths - Subtitle C § 909.2

Subtitle C § 904.2

Access Requirements (for 14%, not 12% driveway grade) - Subtitle C § 909.3

Special Exception Loading Requirement Standards (C § 909.2)

909.2 (b) The loading berths or service/delivery spaces are required for an addition to a historic resource, and providing the required loading facilities would result in significant architectural or structural difficulty in maintaining the integrity and appearance of the historic resource.

- N/A

Special Exception Loading Requirement Standards (C § 909.2)

909.2 (a) The only means by which a motor vehicle could access the lot is from a public street, and provision of a curb cut or driveway on the street would violate any regulation in this chapter, or in Chapters 6 or 11 of Title 24 DCMR; or

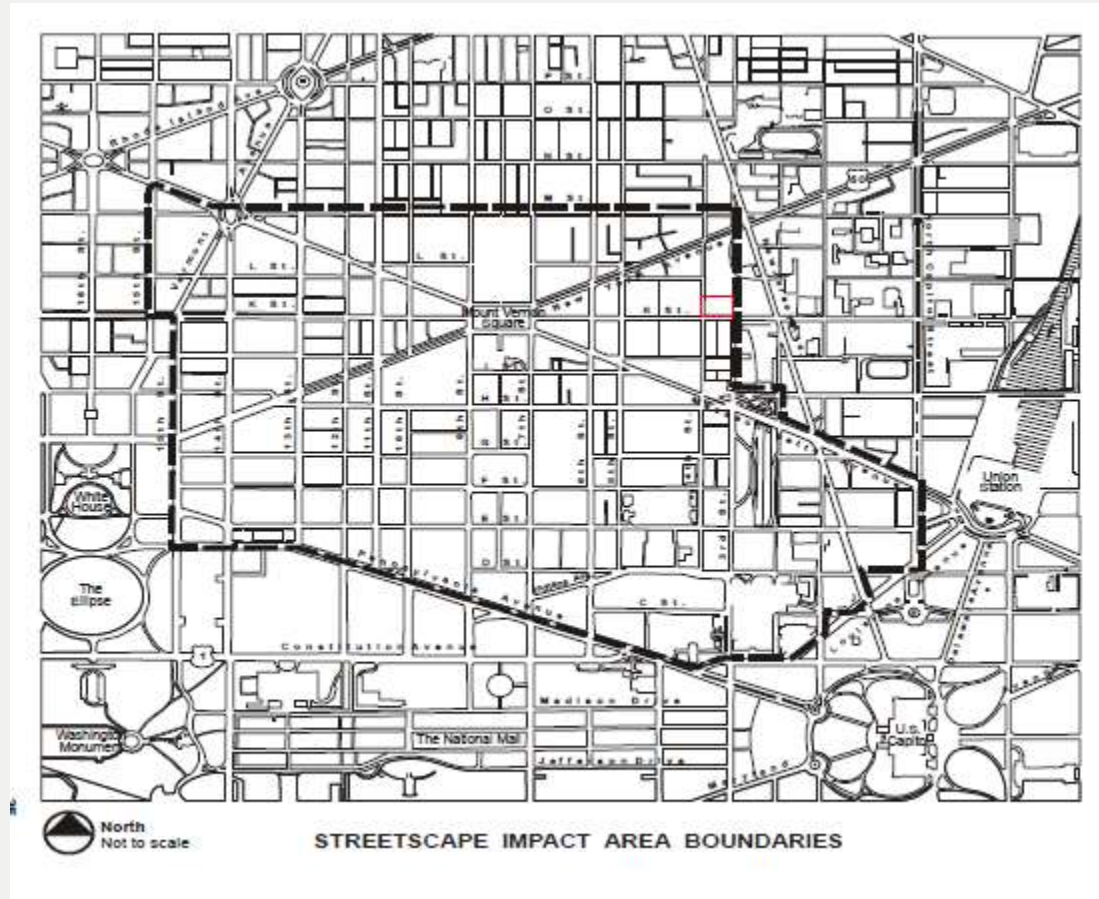
- Property has no adjacent alley
- DDOT has prohibited a curb cut on K Street, NW, making the only possible access point a curb cut from 4th Street, NW
- ZA has determined that, because the proposed curb cut would not conform to 24 DCMR Chapters 6 or 11, it would be appropriate to consider the request as a special exception

Special Exception Standard (§ X-901.2)

- Special exception relief is presumed to be appropriate, reasonable, and compatible with other uses in the same zoning classification, provided the specific requirements for the relief are met. *First Baptist Church of Wash. v. District of Columbia Bd. of Zoning Adjustment*, 432 A.2d 695, 701 (1981).

Proposed 4th Street Curb Cut would violate Chapter 11 of Title 24 DCMR and DDOT's Design and Engineering Manual

- Chapter 11 of Title 24 DCMR (regulations of the Downtown Streetscape) Applies to the Property because it is located within the Downtown Streetscape area



Proposed 4th Street Curb Cut would violate Chapter 11 of Title 24 DCMR and DDOT's Design and Engineering Manual

- DCMR Title 24 § 1110.2 and § 1110.3 includes numerous requirements for curb cut/driveways in the Downtown Streetscape area that cannot be satisfied here, including:

1110.2 The design of a driveway shall fulfill the following requirements:

- (a) Driveways and their aprons shall be poured concrete and flush with grade of sidewalk;
- (b) Driveway shall be a minimum width of twelve feet (12') and a maximum width of twenty-five feet (25');
- (c) The radius for curb returns for driveways shall be six feet (6');
- (d) There must be a minimum six feet (6') wide pedestrian safety island between two driveways that are more than twenty four feet (24') wide. This pedestrian island shall match the same material used for the sidewalk. It shall have a three feet (3') radius at the curb of the street;
- (e) All alleys are required to have a minimum width of twenty feet (20') and radius at curb shall be ten feet (10'). Driveway shall not be located within sixteen feet (16') of another driveway or alley;
- (f) **Driveway shall be at least eight feet (8') from the adjacent interior property line;**
- (g) Driveway shall be at least forty feet (40') from the point of the intersection of the two (2) street curbs;

1110.3 **Driveways shall be located according to the Driveway Location Standards approved by the Director.**

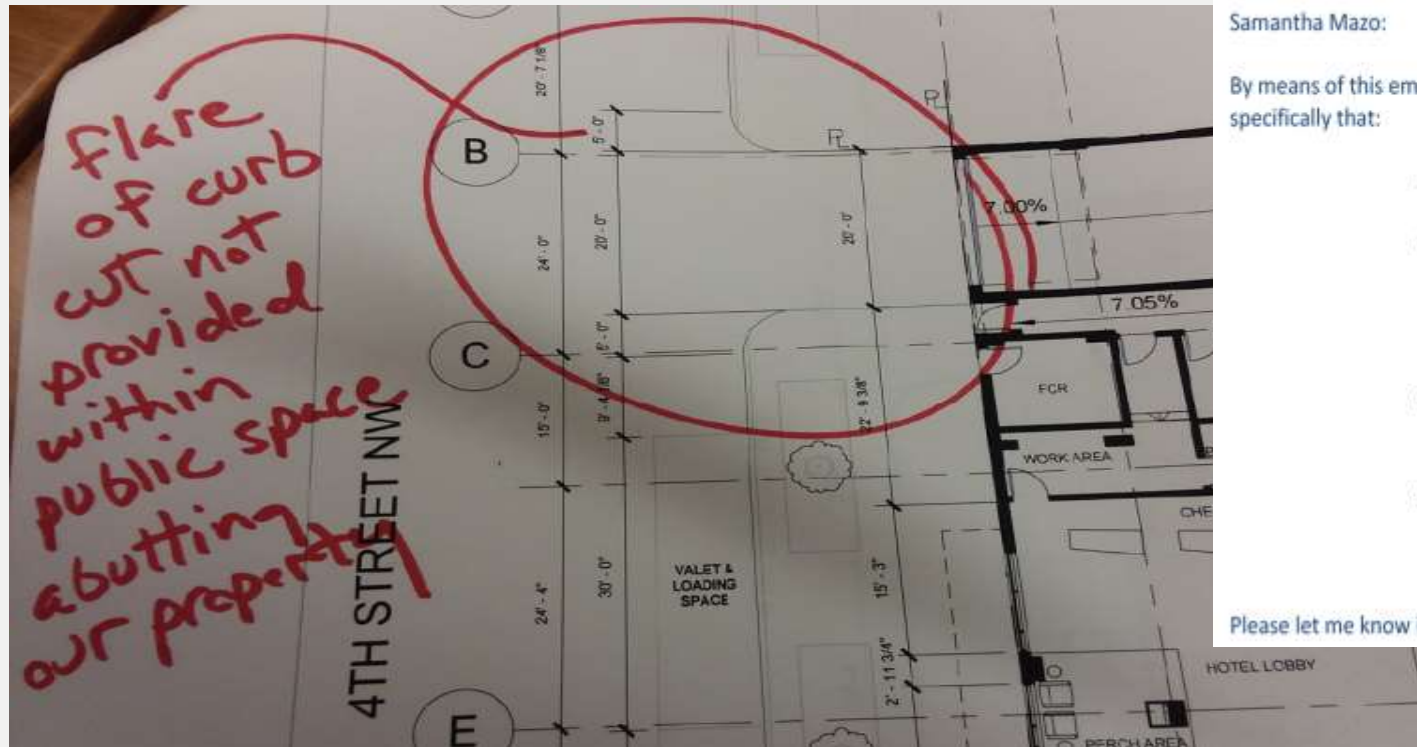
Proposed 4th Street Curb Cut would violate Chapter 11 of Title 24 DCMR and DDOT's Design and Engineering Manual

Proposed 4th Street curb cut also does not satisfy Section 31.5.1 of the DDOT DEM, which states:

- A curb cut and/or respective portion of the driveway, including the flare or radius at the curb cut, must be within the public space abutting the same lot with the building or structure it is intended to serve.

Proposed 4th Street Curb Cut would violate Chapter 11 of Title 24 DCMR and DDOT's Design and Engineering Manual

- DCMR Title 24 § 1110.2(f) not satisfied because the curb cut is on the property line, not eight feet away from it, as required
- Section 31.5.1 of the DDOT DEM is not satisfied because the “flare” of the curb cut extends 5’ into the adjacent public space
- Special Exception relief requirements are satisfied



Samantha Mazo:

By means of this email I agree with the analysis and summary in the below email, and as shown in the attachments, and specifically that:

- The Project proposes a new curb cut from 4th Street.
- Subtitle C § 909.2(a) permits the BZA to grant a special exception if “the applicant demonstrates that: The only means by which a motor vehicle could access the lot is from a public street, and provision of a curb cut or driveway on the street would violate any regulation in this chapter, or in Chapters 6 or 11 of Title 24 DCMR.”
- The proposed curb cut would not conform to the DCMR Title 24 Chapter 11, or other requirements of DDOT's Design and Engineering Manual (“DDOT's DES”).
- Therefore, the requested Modification loading relief can be processed as a special exception because the 4th Street Curb Cut would not be in compliance with either Title 24 Chapter 11 or DDOT's DES.

Please let me know if you have any further questions

Special Exception Under C § 909.3 from the Access Requirements of C §§ 904.2 and 904.3

□ 14% slope provided where a 12% grade is required

909.3 (a) The lot has unusual topography, grades, shape, size, or dimensions; or

- The lot has unusual shape and is not rectangular. The eastern portions of the lot extend out farther than the western portions, creating an unusual “L” Shape
- It is a corner lot in the Downtown Streetscape area. No curb cuts are possible along K Street, and 4th Street curb cut is subject to DCMR Title 24 Chapter 11 and DDOT’s Design and Engineering Manual
- Fully-compliant 12% grade would not permit two service spaces to be located within in the below-grade garage

Special Exception Under C § 909.3 from the Access Requirements of C §§ 904.2 and 904.3

909.3 (b) Alternate access arrangements would improve site design, landscaping, or traffic patterns or provide safer ingress or egress.

- Requested slope would enable the applicant to accommodate both parking and two service spaces from 4th Street, with a single driveway and ramp system
- No curb cut from K Street is permitted. Even if one were permitted, it is not preferable because having no K Street curb cut avoids introducing additional mid-bock turning movements and respects the public space landscaping plan for K Street

Loading Relief is Harmonious with Purpose & Intent of ZR and Maps

- The purposes of the D zones are to promote the development of high-density developments, including hotels
- The Downtown Streetscape plan does not permit curb cuts on K Street. Accordingly, the curb cut must be from 4th Street, which does not accommodate the full front-in/front-out loading movements required by DDOT for a 30'-berth
- The currently-proposed two service spaces will accommodate the necessary loading needs from the 4th Street curb cut in a manner harmonious with the Zoning Regulations
- In the 2016 Approval, the Board found that relief from the 30'-loading berth requirement would not substantially impair the intent, purpose and integrity of the Zone Plan
- The Board's 2016 Approval was supported by OP and DDOT
- Current relief is also supported by OP and DDOT

No Adverse Effect on Use of Neighboring Properties

- With the 14% driveway grade, Applicant will provide two 20'-service spaces, which are anticipated to accommodate most deliveries to the Property
 - DDOT: “A well-designed service delivery space would be able to accommodate a portion of the site’s loading needs, thereby reducing demand for curbside loading space.”
- Expected minimal number of larger-truck daily trips can be accommodated on 4th Street in the anticipated 50' dedicated loading/valet area
- The removal of the residential use from the Project likely reduces the need for large truck loading (no residential “move-ins” or “move-outs”)
- If additional large loading is required, can be accommodated in the commercial loading zone in front of the Ellisdale project
- Loading Management Plan can be made a separate Condition of Approval to this Application

Loading Management Plan (Items 1-4)

1. A loading coordinator will be on duty during delivery hours, and vendors will be required to coordinate and schedule deliveries
2. Trucks accessing the on-street loading space will be limited to a maximum of 30' in length.
3. Any loading conducted using a truck greater than 20' in length must be scheduled with the loading coordinator
4. Deliveries will be scheduled such that the on-street loading capacity is not exceeded and so as not to conflict with potential valet operations. In the event that an unscheduled delivery vehicle arrives while the loading space is full, that driver will be directed to return at a later time when the loading space will be available so as to not impede traffic along 4th Street

Loading Management Plan (Items 5-8)

5. Deliveries from 30'-trucks or 20'-service vehicles will be prohibited from being delivered directly from K Street and instead will be required to use the loading area on 4th Street
6. Trucks using the loading area will not be allowed to idle and must follow all District guidelines for heavy-vehicle operation including but not limited to DCMR 20 – Chapter 9, Section 900 (Engine Idling), the regulations set forth in DDOT's Freight Management and Commercial Vehicle Operations document, and the primary access routes listed in the DDOT Truck and Bus Route System
7. The loading area operation will be limited to daytime hours of operation, with signage indicating these hours posted prominently at the loading space with notification also given to tenants. The use of the on-street space for loading will be coordinated with valet operations so as to determine the most optimal times for loading
8. Any delivery using a truck 20' in length or shorter will be required to utilize the on-site delivery space

Community Outreach

- Applicant presented the Project at the May 31, 2018, ANC 6E Development and Zoning Subcommittee meeting; Subcommittee recommended approval by the full ANC
- Applicant presented the Project at the full ANC 6E meeting on June 5, 2018
 - Unanimous vote in support from the ANC (6-0-0)
 - Community questions were raised about rat abatement
 - Community was informed that internal trash room is proposed
 - Applicant agreed to take steps to address neighbor concerns about rats in the area
- DDOT Support for the Application: BZA Exhibit No. 35
- OP Support for the Application: BZA Exhibit No. 38

Back of Deck

2016 Approval – Cover sheet

317 K STREET, NW
Square: 526 Lots: 20, 21, 804, 805, 824, 829

LIST OF DRAWINGS:

- A-00 COVER SHEET
- A-01 ZONING ANALYSIS
- A-02 AERIAL VIEW
- A-03 ZONING MAP
- A-04 COMPREHENSIVE PLAN
- A-05 CONTEXT PHOTOS
- A-06 CONTEXT PHOTOS
- A-07 CONTEXT PHOTOS
- A-08 SITE PLAN
- A-09 GROUND FLOOR PLAN
- A-10 TYPICAL HOTEL FLOOR PLAN (FLOORS 2-11)
- A-11 TYPICAL RESIDENTIAL FLOOR PLAN (FLOORS 12-14)
- A-12 ROOF PLAN
- A-13 GARAGE PLAN- G1
- A-14 GARAGE PLAN- G2
- A-15 BUILDING SECTION
- A-16 BUILDING SECTION
- A-17 RENDERED BUILDING PERSPECTIVE



317 K STREET NW

K St, NW
Washington, DC 20024
Square: 526 Lot Nos: 20-21, 804-805, 824-825, 829



PGN Architects, PLLC
210 7th Street SE - Suite 2011
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COVER SHEET | A-00

02-11-2016

2016 Approval – Zoning data

ZONING REQUIREMENTS	REQUIRED / ALLOWED		PROVIDED	
Zoning District	C-2-C / DD (Downtown Development) / DC-HPA (Housing Priority Area A)		C-2-C / DD (Downtown Development) / DD-HPA (Housing Priority Area A)	
Lot Area	10,767		10,767	
FAR Total -hotel /Residential (Max. for Preferred uses 11)	11.0	117,997	11.0	
Inclusive FAR Breakout*				
Non-Residential FAR (-hotel)	3.5	37,685	3.5	
Off Site CLD Residential (-hotel)	3.5	37,685	3.5	
Financial Contribution to Affordable Housing (-hotel)	1.0	10,767	1.0	
Subtotal (-hotel)	8.0	86,136	8.0	
Bonus Financial Contribution to Affordable Housing or TDRs (-hotel)	0.5	5,384	0.5	
	8.5	91,520	8.5	91,368
Residential FAR Onsite	2.5	26,477	2.5	26,477
TOTAL FAR	11.0	117,997	11.0	117,845
Lot Occupancy				
Ground Floor	100%	10,767	100%	10,767
Typical Hotel Floor (2-11)	100%	10,767	73%	8,407
Typical Residential Floor (12-14)	90%	9,690	77%	8,338
Height	130'-0"		130'-0"	
Closed Court at Hotel Floors 1-11 (-height of Court- 100'-0")	Hotel width 3" per foot but not less than 12'-0". Required width= 25'-0" Minimum area- Not less than 250 SF and not less than twice the square width of required court (25x25) x 2= 1,250 SF		35'-6" x 54'-4" 2,054 SF (irregular shape)	
Closed Court at Residential Floors 12-14 (-height of Court- 30'-0")	Residential width 4" per foot but not less than 15'-0". Required width= 15'-0" Minimum area- Not less than 350 SF and not less than twice the square width of required court (15x15) x 2= 450 SF		35'-6" x 54'-4" 2,054 SF (irregular shape)	
Rear Yard Setback	15'-0"		None provided Relief Needed	
Side Yard Setback	None required.		None	
Penthouse Height	13'-6"		18'-6"	
Hotel Parking (Based on floors 2-11 hotel / 200 rooms and 2,040 hotel/publicspace/bar)	1 space per 2 rooms plus 1 for each 150 SF of floor area for largest function room. 100 + 14= 114		33 Provided Relief Needed for 76	
Residential Parking (Based on floors 12-14 as residential, 30 units total)	1 for each 4 dwelling units= 8 spaces		8 spaces	
Total Parking (Residential + Hotel)	122 spaces		46 Spaces	
Hotel Loading Berth	1 @ 30'-0" Deep		None provided Relief Needed	
Hotel Loading Platform	1 @ 100 S ²		None provided Relief Needed	
Hotel Service/Delivery Loading Space	1 @ 20'-0" Deep		1 @ 20'-0" Deep	
Residential Loading Berth, Platform, Space	None- Less than 50 units		None	
Hotel Bikes	None required.		None	
Residential Bikes	1 space per 3 units. Required 10		34	
GAR	Approx. 30% site area to be vegetated.		Approx. 30% site area to be vegetated.	

* Applicant requests flexibility. Breakdown is a preliminary option to illustrate how maximum FAR could be reached on the property.

FAR and GSF		HOTEL		RESIDENTIAL		FAR	CONSTRUCTION
Floors	Parking Not FAR	Total Sleeping Rooms	Total Area Hotel FAR	Total Residential Units	Total Area Residential FAR	TOTAL	TOTAL
Penthouse							
Fourteen				10	8,338	8,338	8,338
Thirteen				10	8,338	8,338	8,338
Twelve				10	8,338	8,338	8,338
Eleventh		20	8,325		82	8,407	8,407
Tenth		20	8,325		82	8,407	8,407
Ninth		20	8,325		82	8,407	8,407
Eighth		20	8,325		82	8,407	8,407
Seventh		20	8,325		82	8,407	8,407
Sixth		20	8,325		82	8,407	8,407
Fifth		20	8,325		82	8,407	8,407
Fourth		20	8,325		82	8,407	8,407
Third		20	8,325		82	8,407	8,407
Second		20	8,325		82	8,407	8,407
First			8,118		643	8,761	8,761
Garage 1	10,767						10,767
Garage 2	10,767						10,767
Total	21,534	200	91,368	30	26,477	117,845	139,379

317 K STREET NW

K St, NW
Washington, DC 20024



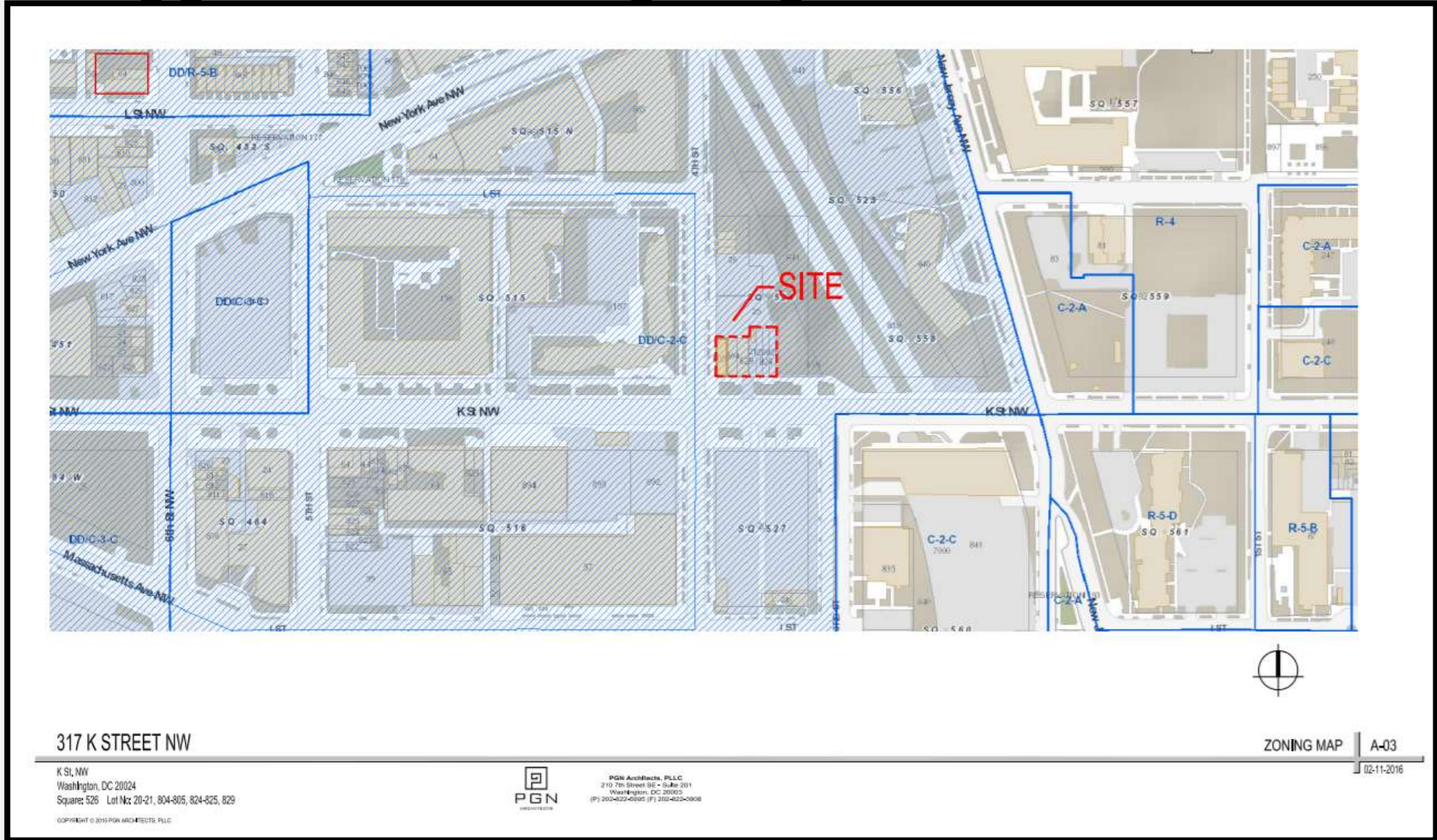
PQR Architects, PLLC
210 7th Street SE - Suite 201
Washington, DC 20003

ZONING DATA | A-01

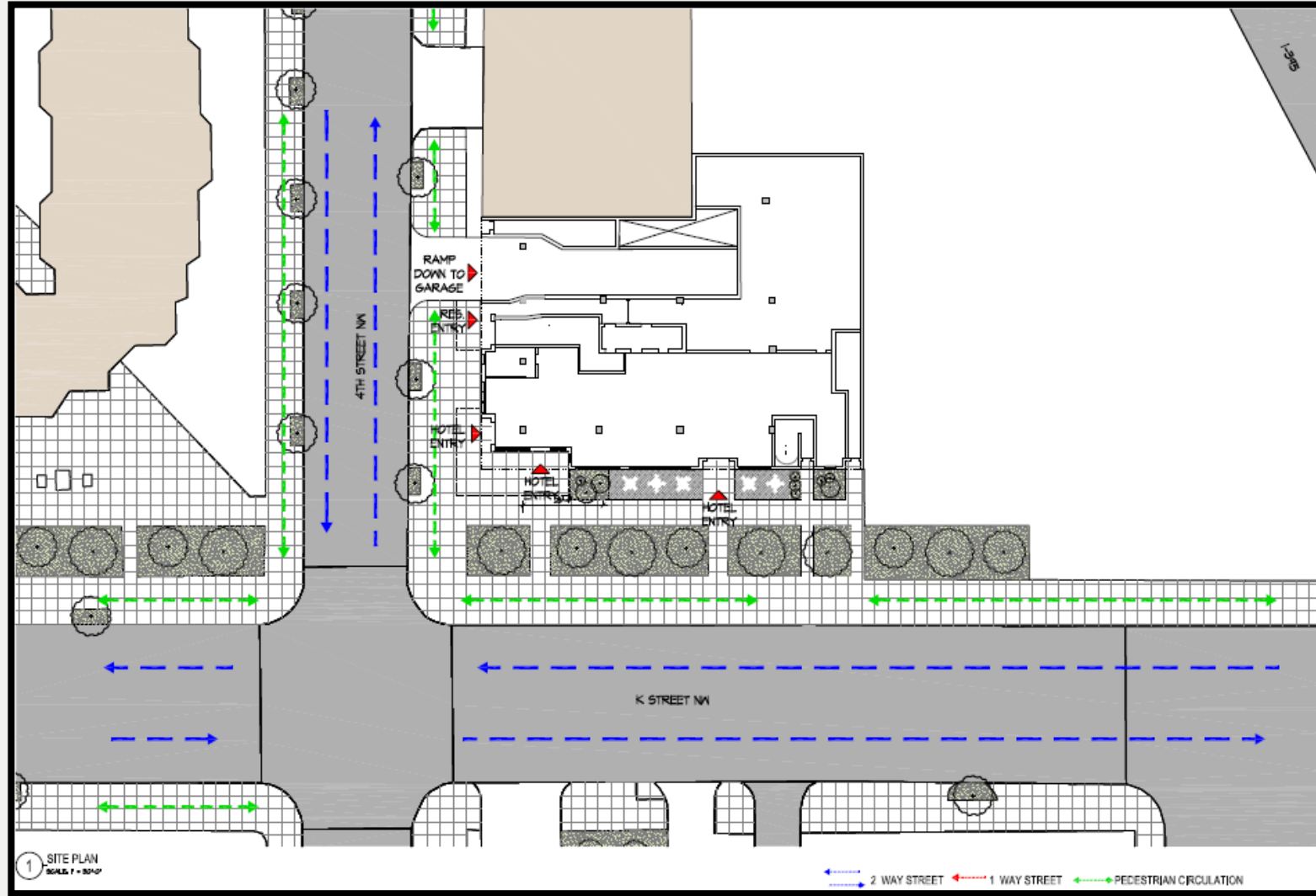
02-11-2016



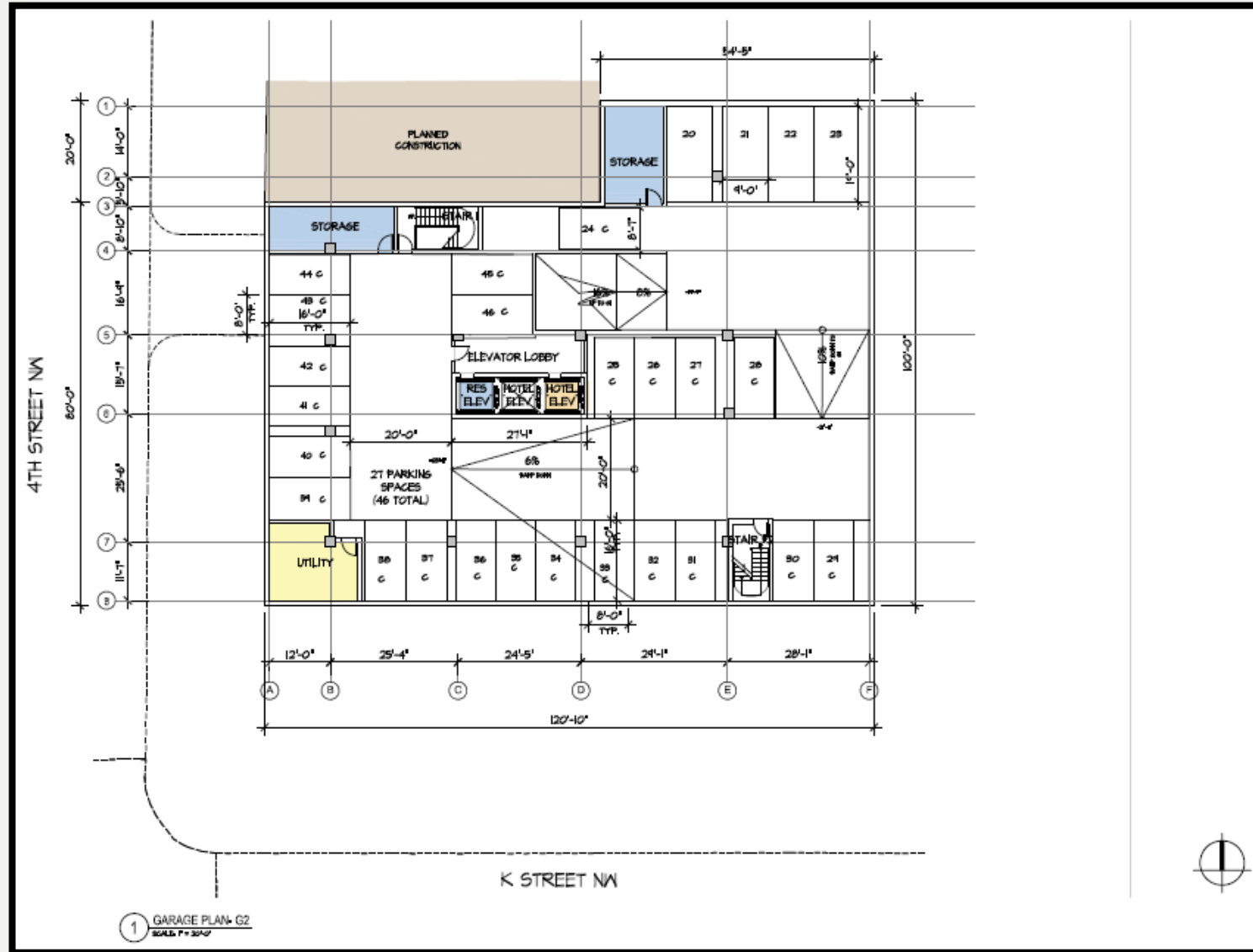
2016 Approval – Zoning map



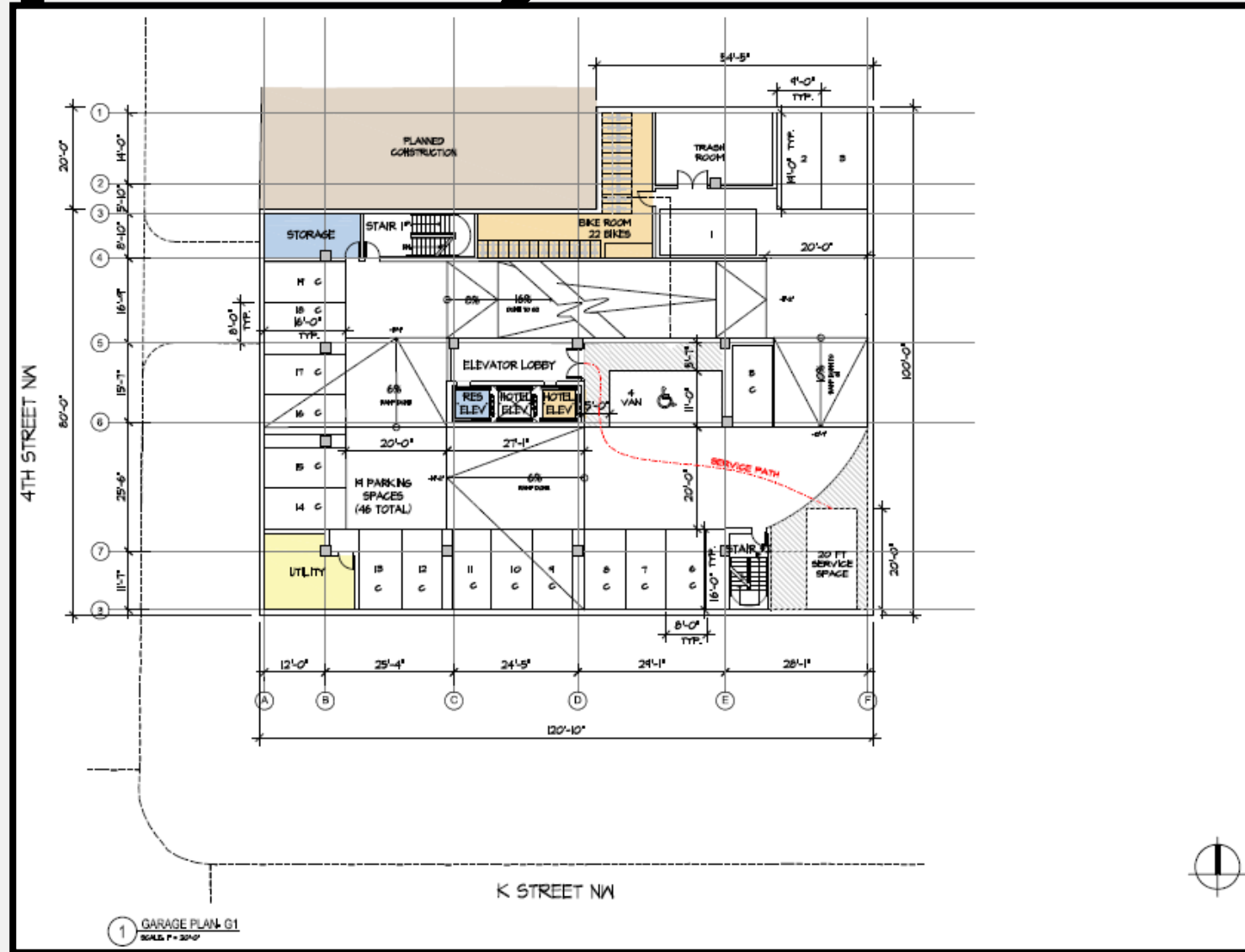
2016 Approval – Site Plan



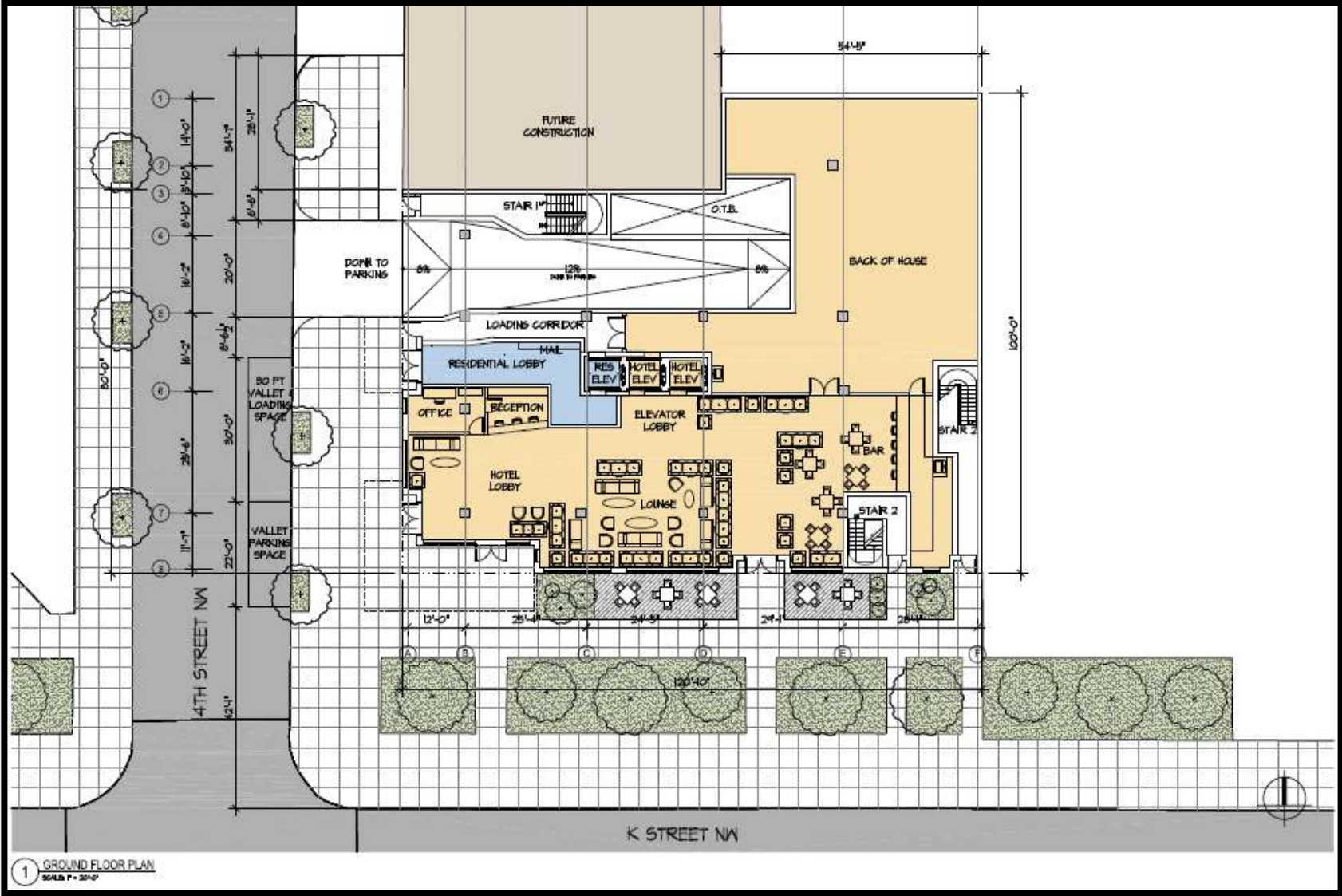
2016 Approval – Garage G2



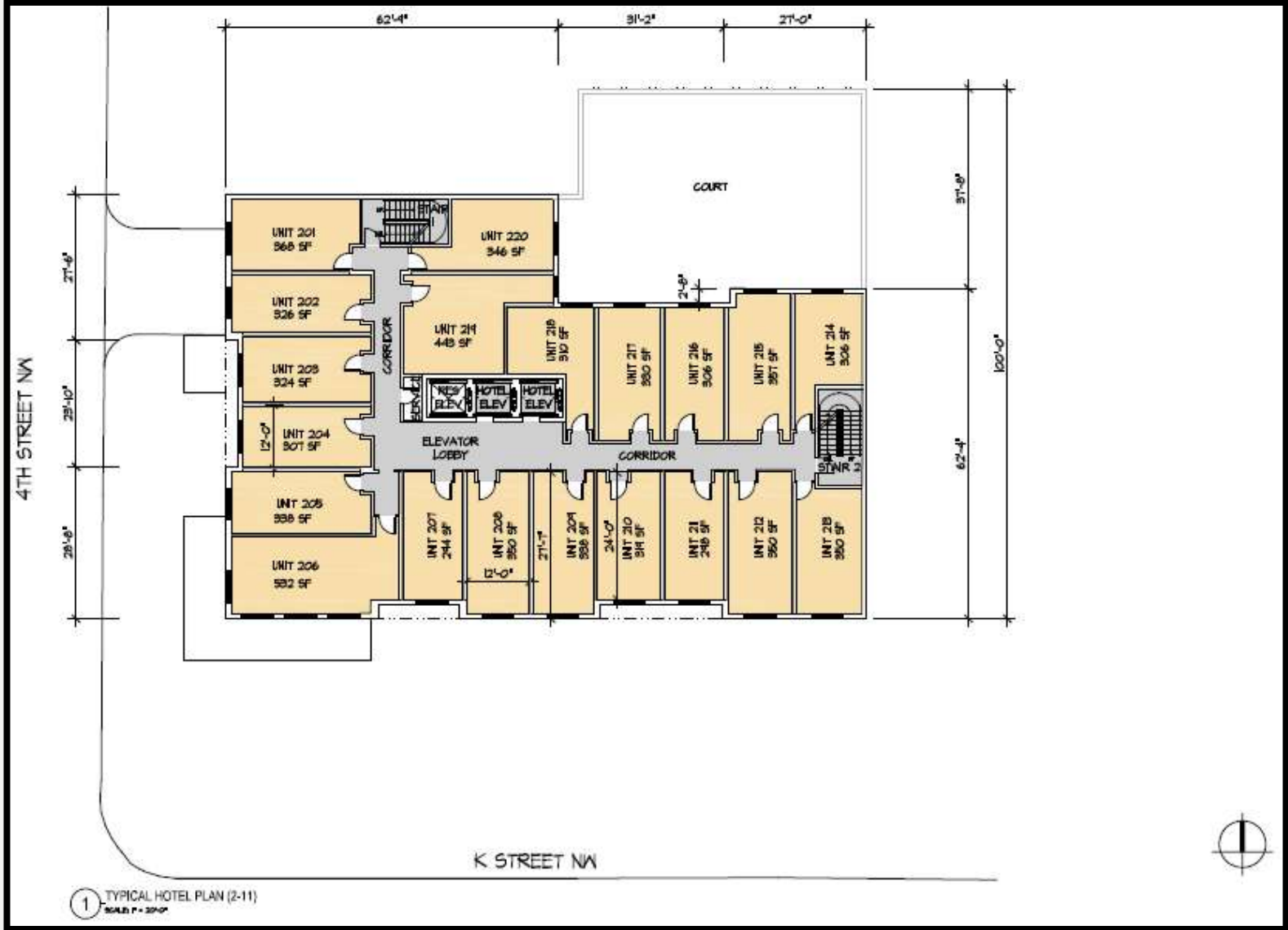
2016 Approval – Garage G1



2016 Approval – First Floor



2016 Approval – Floors 2 - 11

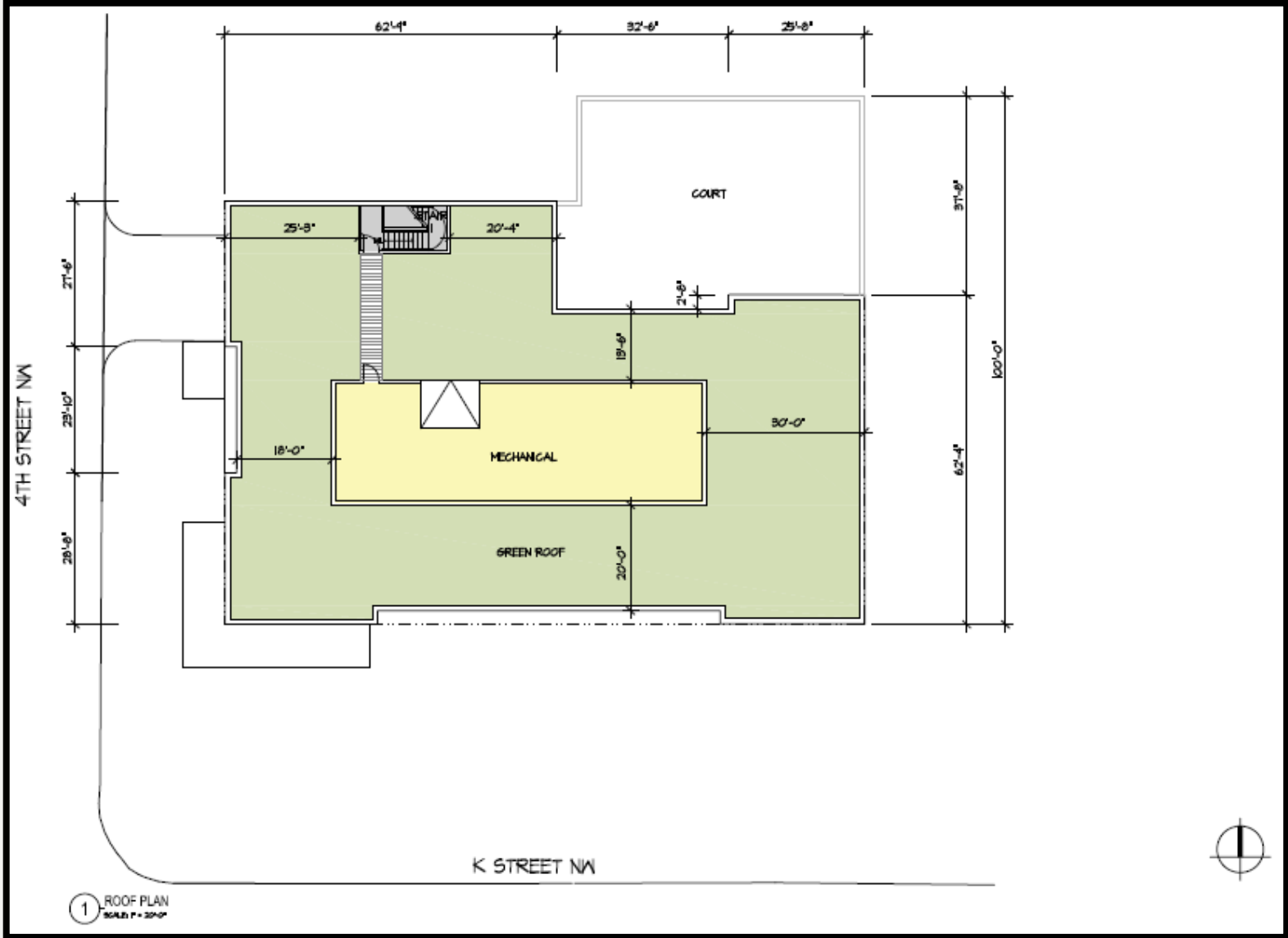


1 TYPICAL HOTEL PLAN (2-11)
SCALE: 1" = 20'-0"

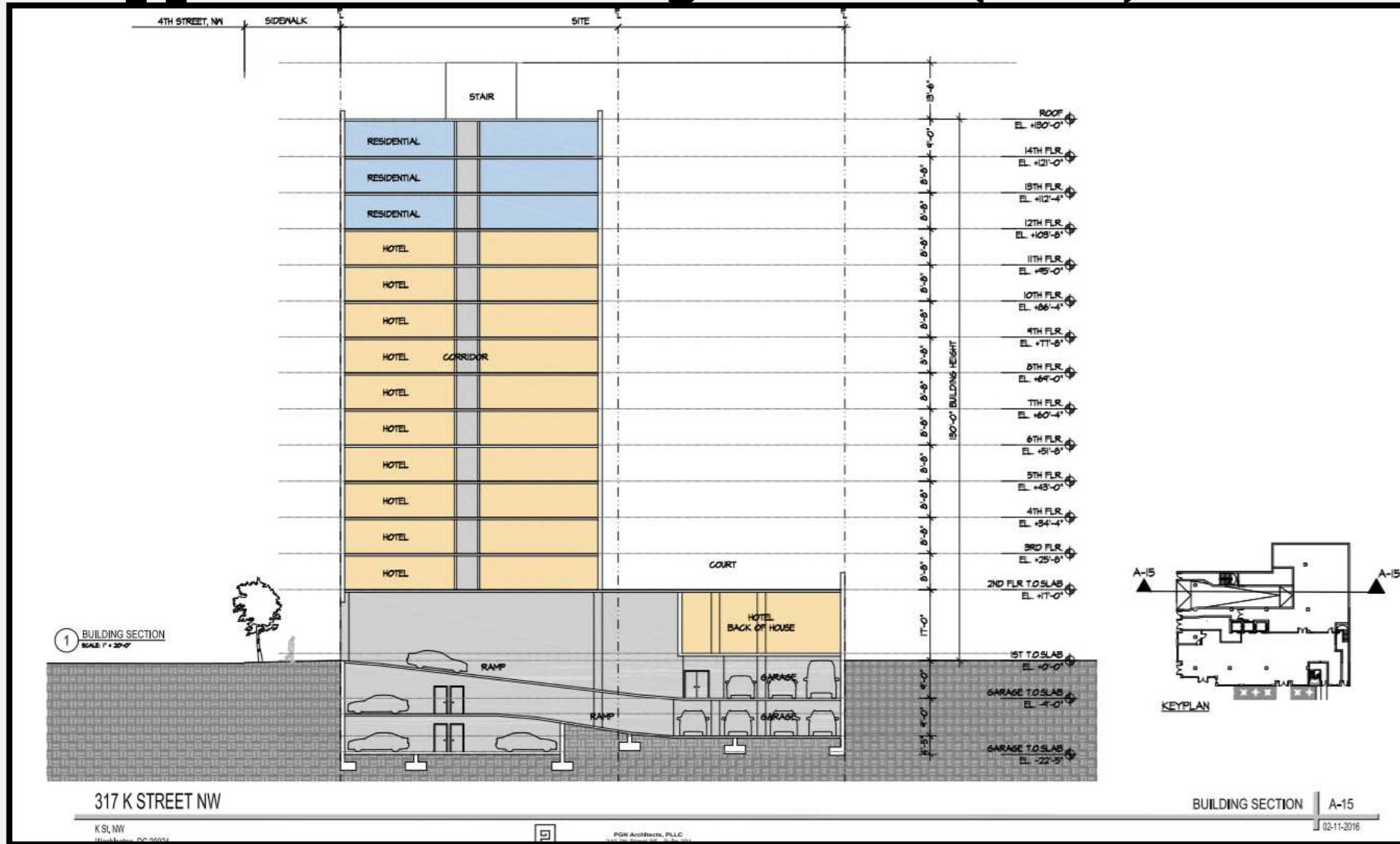
2016 Approval – Apartment Floors 12 - 14



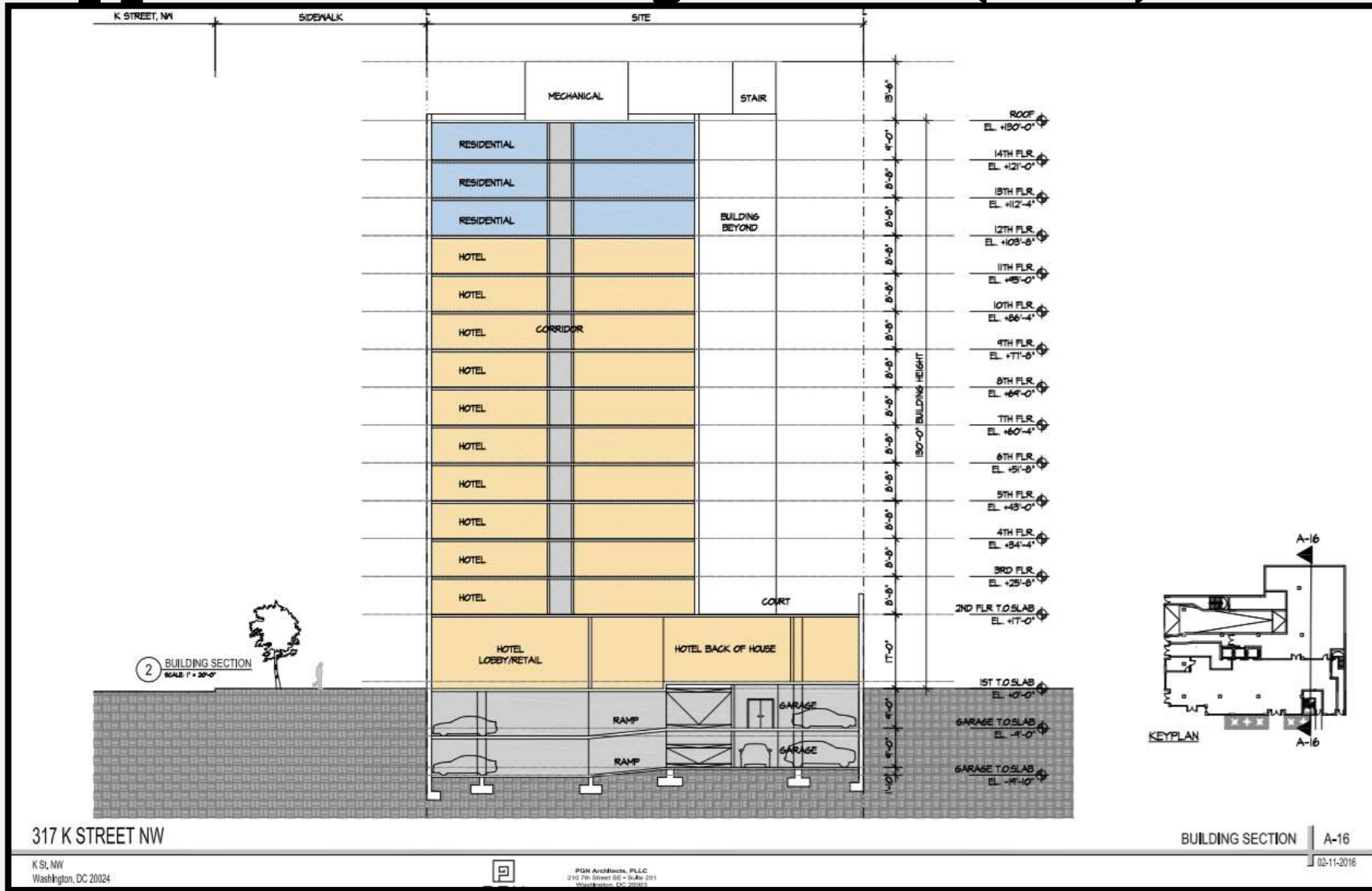
2016 Approval – Roof



2016 Approval – Building Section (A-15)



2016 Approval – Building Section (A-16)



Modified Project Plans – Cover Sheet

317 K STREET NW

317 K STREET NW
WASHINGTON, DC 20001

SQUARE: 526
LOTS: 20, 21, 804, 805, 824, 829

DRAWING LIST

A-00	COVER SHEET
A-01	ZONING DATA
A-10	SITE PLAN
A-11	GROUND FLOOR PLAN
A-12	SECOND FLOOR PLAN
A-13	TYPICAL FLOOR PLAN (3-12)
A-14	THIRTEENTH FLOOR PLAN
A-15	ROOF FLOOR PLAN
A-16	PENTHOUSE ROOF PLAN
A-17	GARAGE PLAN - G1
A-18	GARAGE PLAN - G2
A-20	BUILDING SECTION - EASTWEST
A-21	BUILDING SECTION - NORTH/SOUTH



317 K Street NW

COVER SHEET

A-00

Proposed Set – Zoning Data

ZONING REQUIREMENTS	ZR-16 REGULATIONS			
	REQUIRED/ALLOWED		PROVIDED	
Zoning District	D-4-R		D-4-R	
Lot Area	10,758		10,758	
FAR Total Hotel (Max for Preferred uses 11)	11	117,997	10.4	111,604
Inclusive FAR Breakout				
Non-Residential FAR (Hotel)	3.5	37,685	3.5	37,685
Off Site CLD/ TDR/ Credits	7.5	80,312	6.9	73,919
Financial contribution to Affordable Housing (Hotel)	N/A		N/A	0
Subtotal (Hotel)	11.0	117,997	10.4	111,604
Bonus Financial Contribution to Affordable Housing or TRDs (Hotel)			0.0	
Residential FAR onsite	11.0	117,997	10.4	111,604
TOTAL FAR	11.0	117,997	10.4	111,604
Lot Occupancy				
Ground Floor	100%	10,767	100%	10,758
Second Floor	100%	10,767	80%	8,582
Typical Hotel Floor (3-12)	100%	10,767	79%	8,492
Thirteenth Floor			79%	8,472
Height	130'-0"		130'-0"	
Closed Court at Hotel Floors 1-11 (Height of Court - 100')	Current: Hotel width 3" per foot but not less than 12'-0". Required Width=25'0". Minimum area - Note less than 150 SF and not less than twice the square width of required court (25x25) x 2 = 1,250 SF Revised: Hotel use increased. Hotel Floors 1-13 (Height of Court - 110'-0" = 27'-6" min area 1513 sf)		Approx. 35'-4" X 54'-4" (Irregular shape) = 2,166 SF	
Closed Court at Residential Floors 12-14 (Height of Court - 30')	Residential width 4" per foot but not less than 15'-0". Required width=15'0". Minimum area - Note less than 330 SF and not less than twice the square width of required court (15x15) x 2 = 450 SF		Residential Use Removed	
Rear Yard Setback	15'-0" - Not required if closed court's provided		None Provided Rear yard relief approved in BZA Case No. 19169	
Side Yard Setback	None required		None	
Penthouse Height	18'-6"		18'-6"	
Hotel Parking (Based on floors 2-11 hotel/200 rooms and 2,040 hotel public space/bar)	No parking spaces required under ZR-16		42 spaces provided Relief already granted for 76 spaces in BZA Case No. 19169. No additional relief required	
Residential Parking (Based on floors 12-14 as residential, 30 units total)	N/A		Residential Use Removed	
Total Parking	No parking spaces required under ZR-16		42 Spaces	
Hotel Loading Berth	Current: 1 @ 30'-0" Deep Revised: Hotel use increased = 2 @ 30'-0" Deep		None Provided Relief from 30' berth approved in BZA Case No. 19169. Additional relief required for two 30' loading berths.	
Hotel Service/Delivery Loading Space	1 @ 20'-0" Deep		2 @ 20'-0" Deep	
Hotel Bikes	Long Term: 1/10,000sf = 11 bike spaces Short Term: 1/40,000sf = 3 bike spaces		14 Bike Spaces Provided	
GAR	Approx. 30% site area to be vegetated		Approx. 30% site area to be vegetated	
Driveway Grade	12% grade required		14% grade proposed - Relief Requested	

- RESIDENTIAL USE REMOVED
- HOTEL USE INCREASED
- FAR REDUCED
- 1 FLOOR REMOVED

FLOORS	KEYS	FAR and GSF		
		NON-FAR	FAR	GSF
Garage 2		10,757 SF		10,757 SF
Garage 1		10,757 SF		10,757 SF
Ground			9,456 SF	9,456 SF
Second	16 KEYS	218 SF	8,582 SF	8,800 SF
Third	21 KEYS	218 SF	8,492 SF	8,710 SF
Fourth	21 KEYS	218 SF	8,492 SF	8,710 SF
Fifth	21 KEYS	218 SF	8,492 SF	8,710 SF
Sixth	21 KEYS	218 SF	8,492 SF	8,710 SF
Seventh	21 KEYS	218 SF	8,492 SF	8,710 SF
Eighth	21 KEYS	218 SF	8,492 SF	8,710 SF
Ninth	21 KEYS	218 SF	8,492 SF	8,710 SF
Tenth	21 KEYS	218 SF	8,492 SF	8,710 SF
Eleventh	21 KEYS	218 SF	8,492 SF	8,710 SF
Twelfth	21 KEYS	218 SF	8,492 SF	8,710 SF
Thirteenth	21 KEYS	117 SF	8,472 SF	8,589 SF
PH Roof		2,862 SF		2,862 SF
PH Mezzanine		223 SF		223 SF
TOTAL	247 KEYS	27114 SF	111,430 SF	138,544 SF

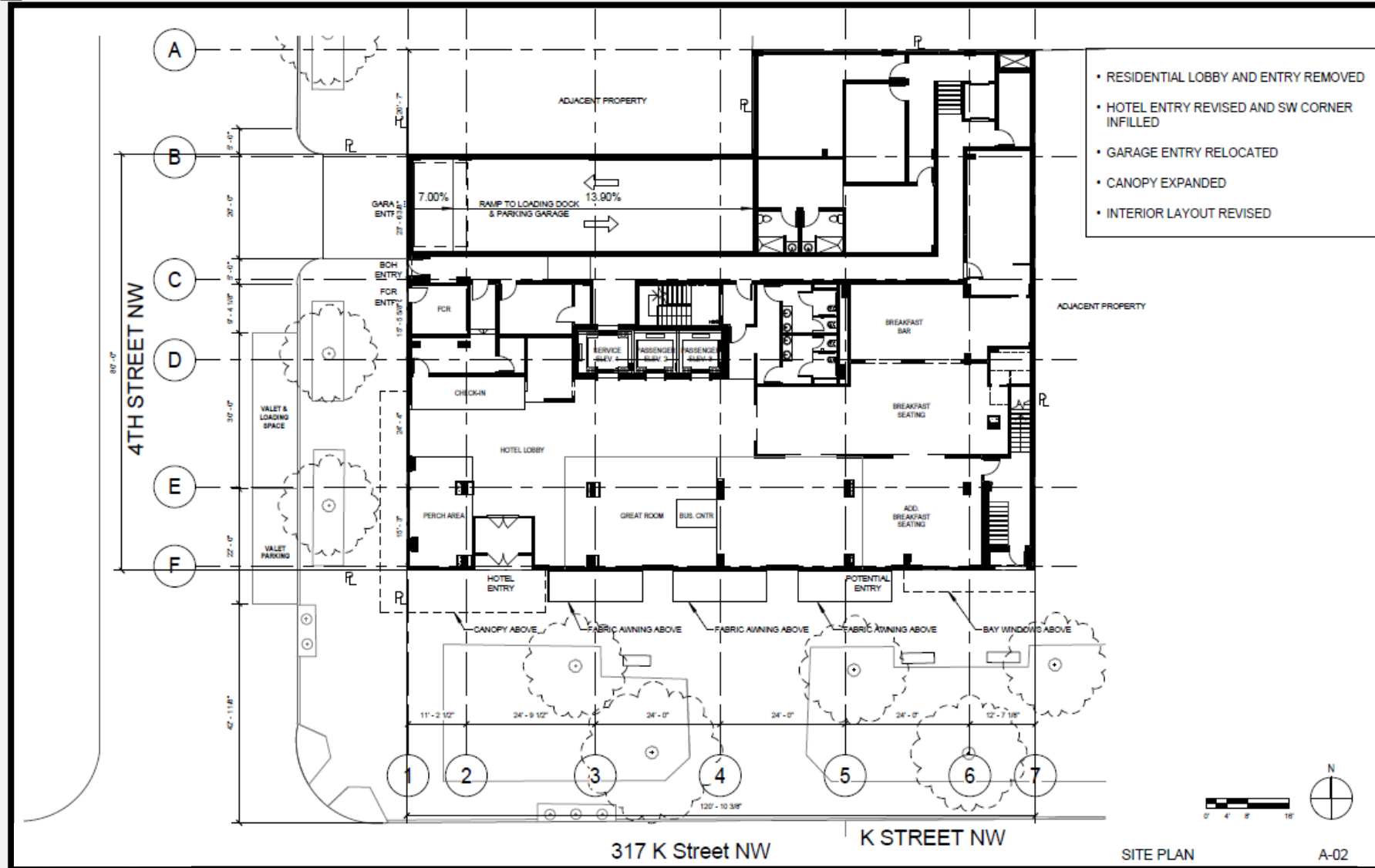
317 K Street NW

ZONING DATA

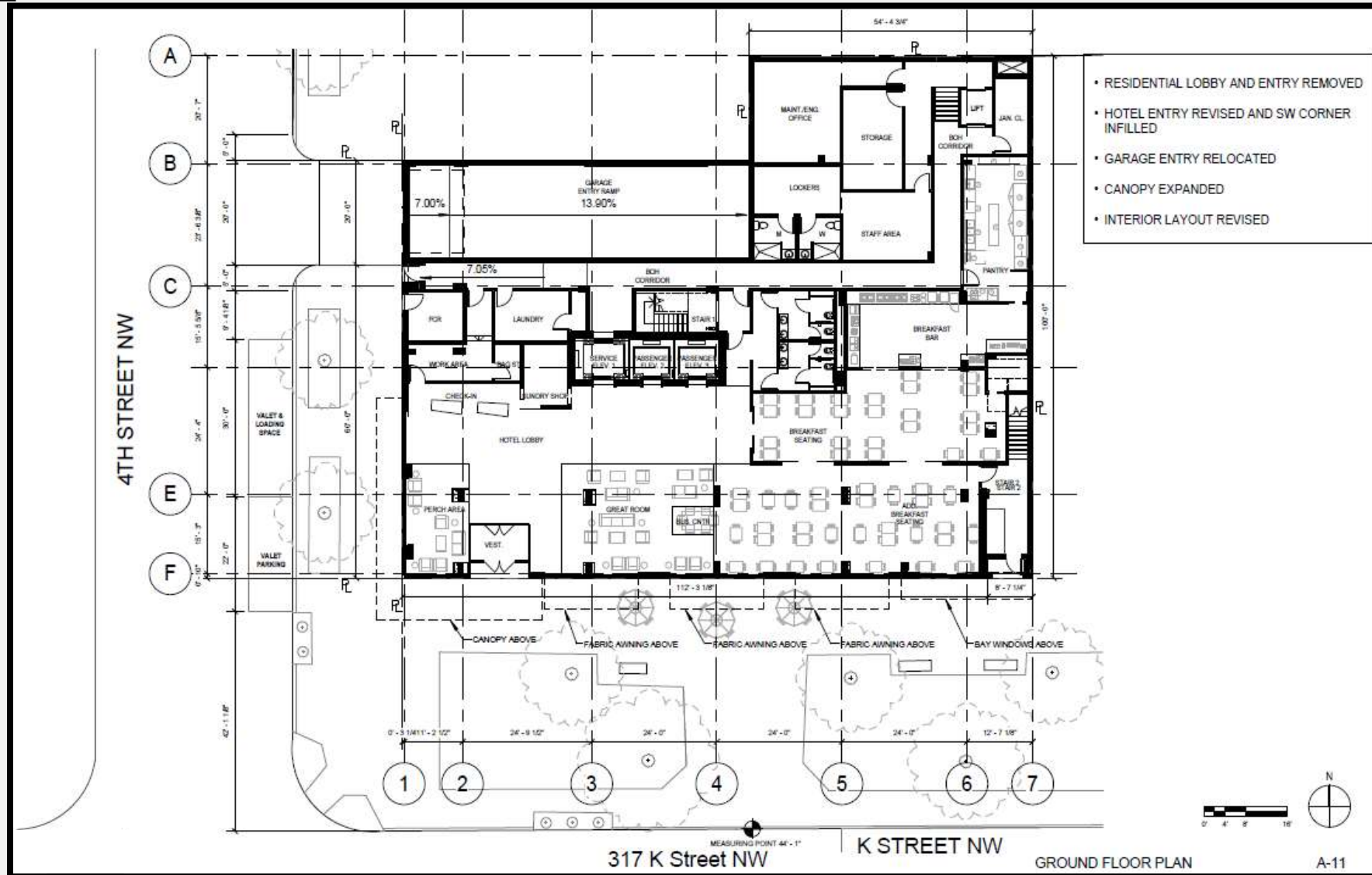
A-01



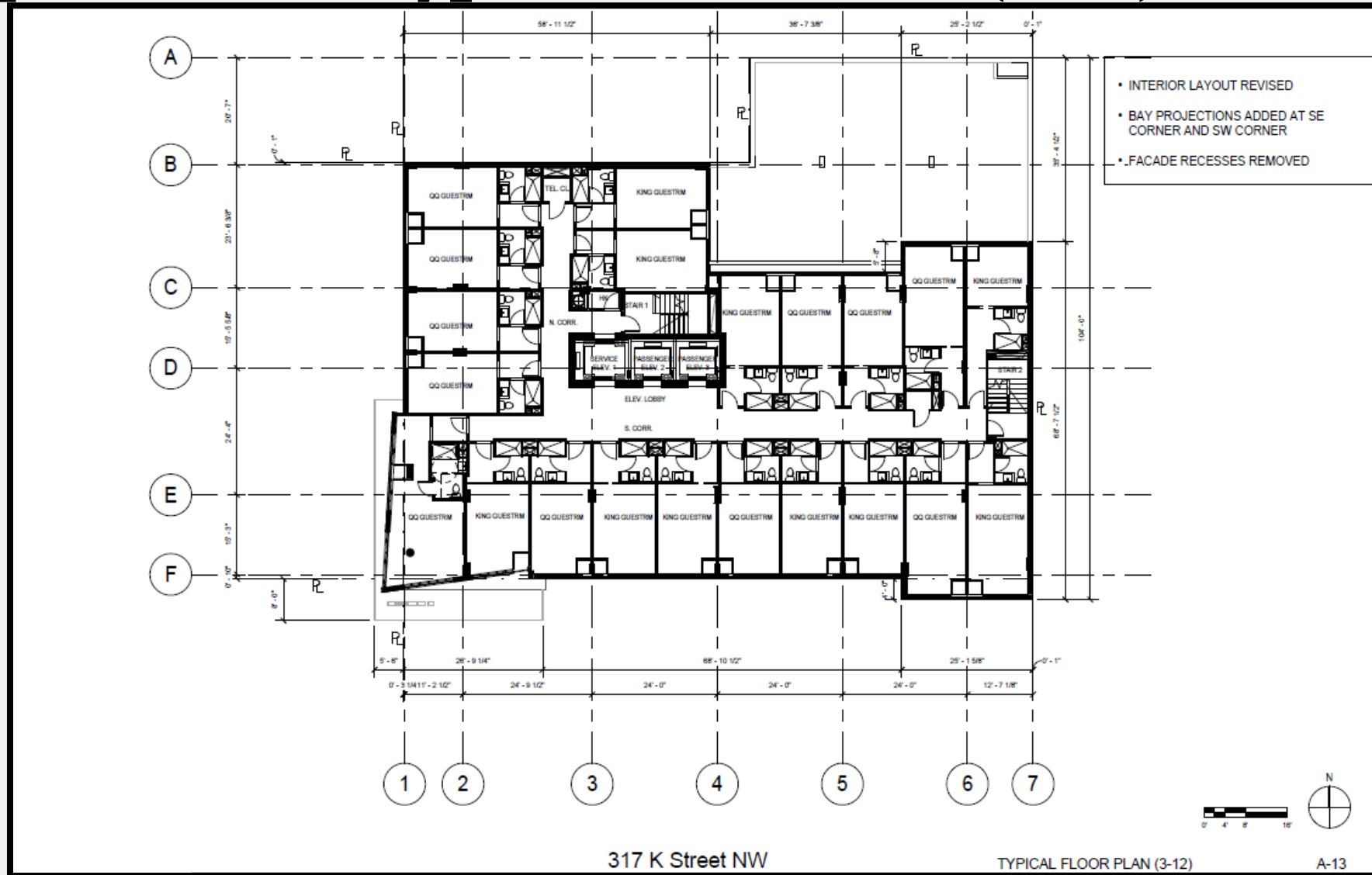
Proposed Set – Site Plan



Proposed Set – Ground Floor Plan



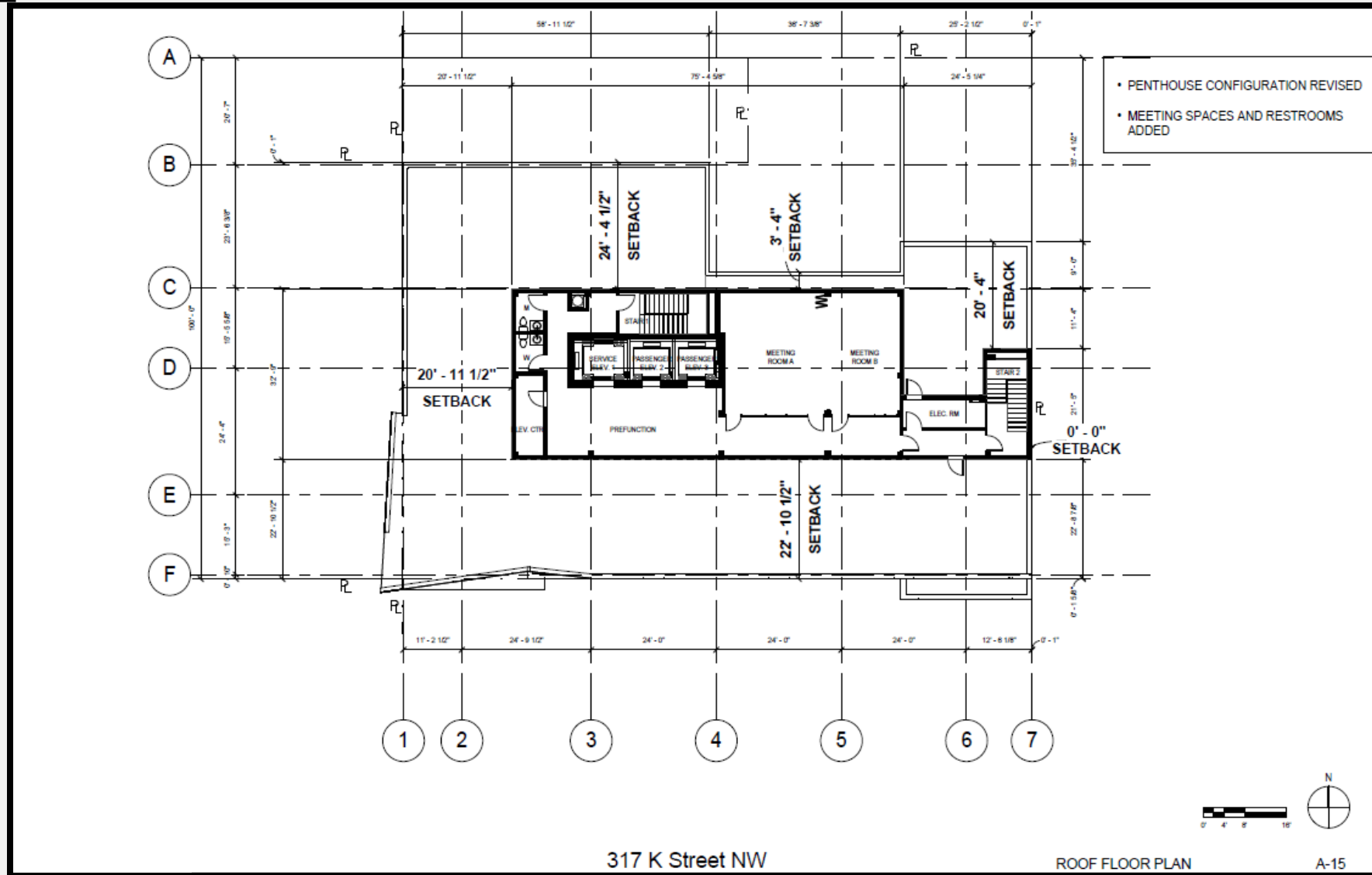
Proposed Set – Typical Floor Plan (3-12)



Proposed Set – Thirteenth Floor Plan



Proposed Set – Roof Floor Plan

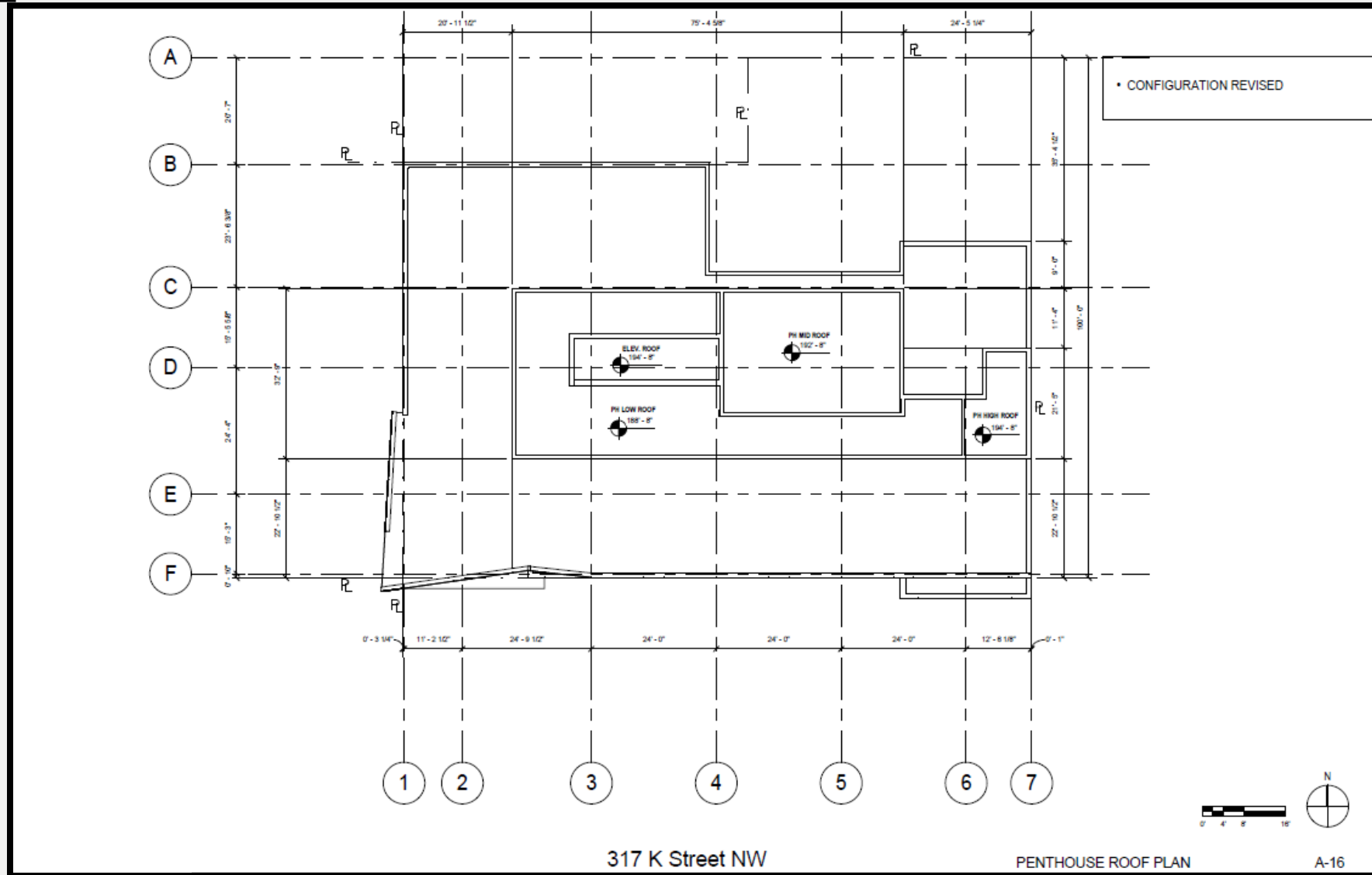


317 K Street NW

ROOF FLOOR PLAN

A-15

Proposed Set – Penthouse Roof Plan

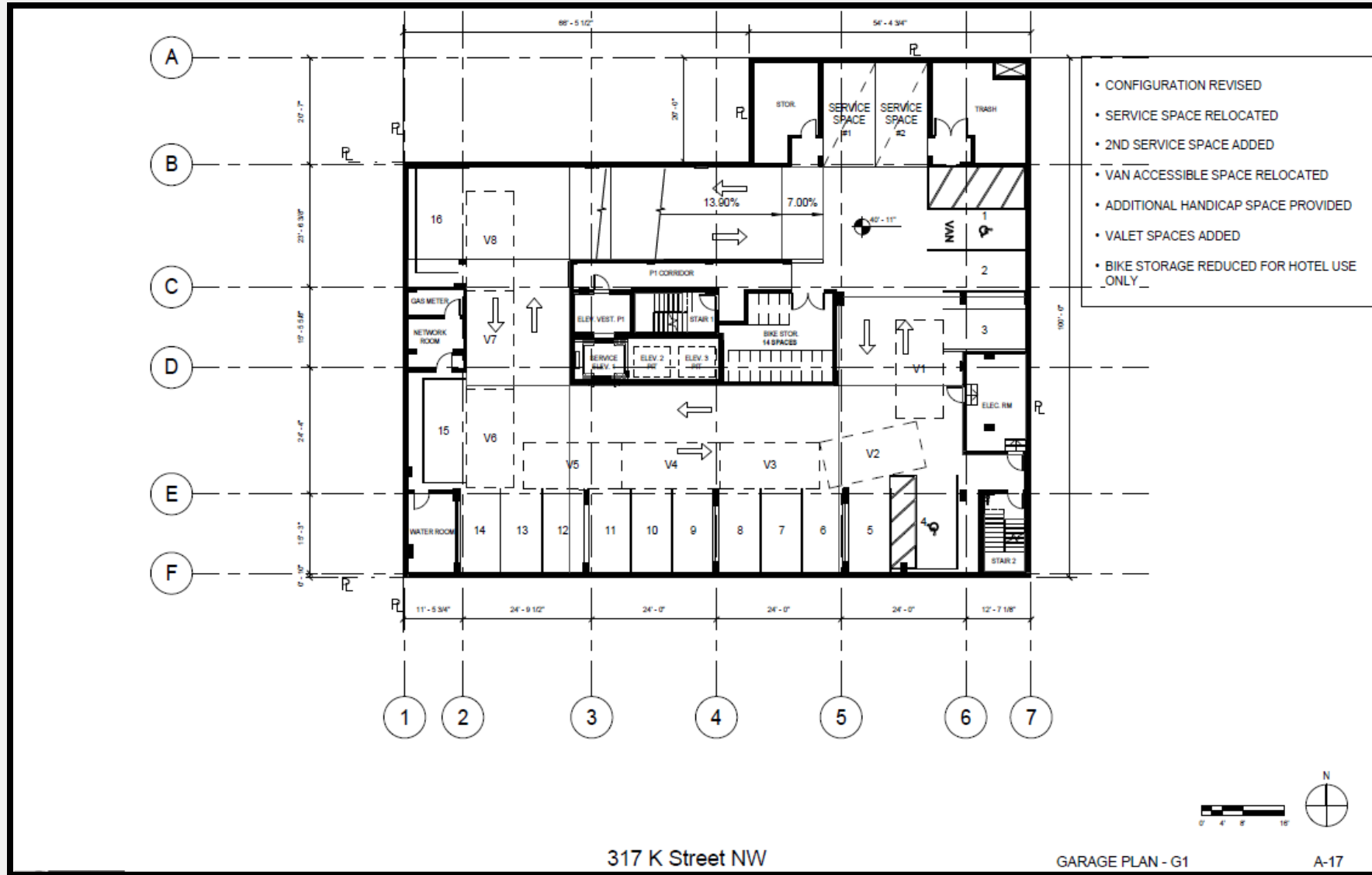


317 K Street NW

PENTHOUSE ROOF PLAN

A-16

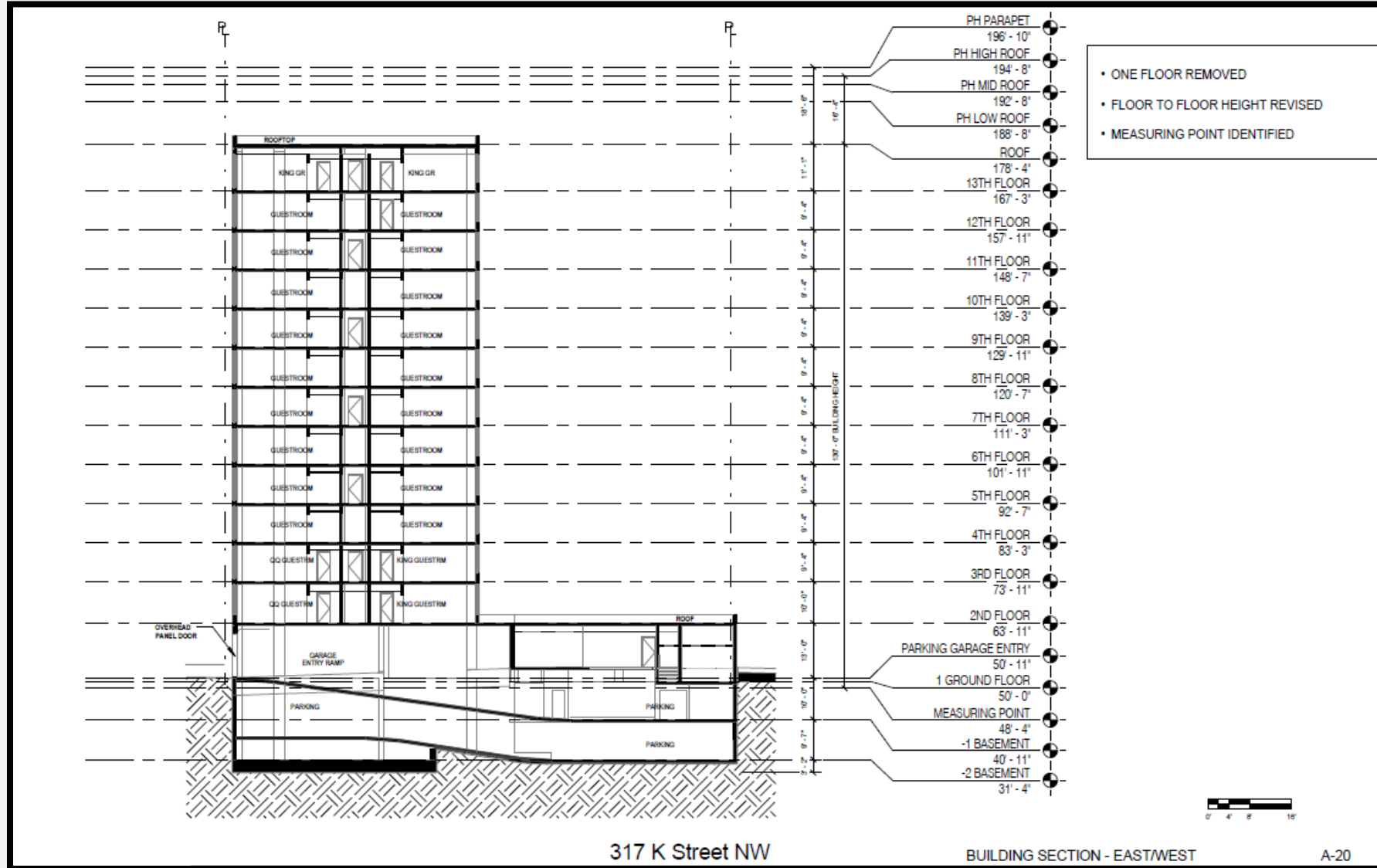
Proposed Set – Garage Plan, G1



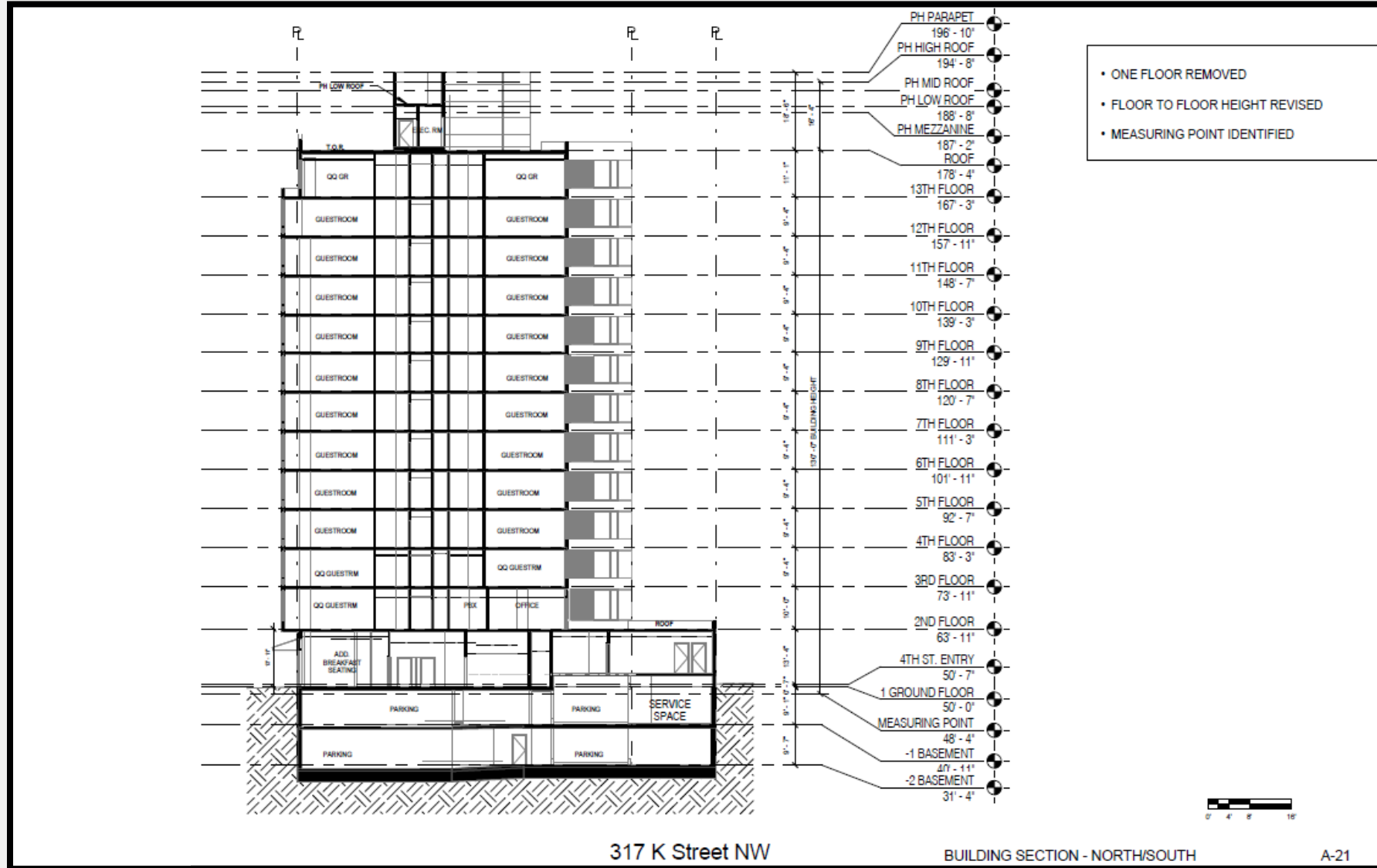
Proposed Set – Garage Plan, G2



Proposed Set – Building Section, East/West



Proposed Set – Building Section, North/South



The Property – 303-317 K Street NW

